

Attachment B



City of Phoenix PLANNING AND DEVELOPMENT DEPARTMENT

Staff Report Z-72-21-7 January 13, 2023

Estrella [Village Planning Committee](#)
Meeting Date:

January 17, 2023

[Planning Commission](#) Hearing Date:

February 2, 2023

Request From:

[RE-35](#) (Single-Family Residence District) (69.00 acres), [R1-8](#) (Single-Family Residence District) (0.42 acres), [C-2](#) (Intermediate Commercial) (0.46 acres)

Request To:

[R-2](#) (Multifamily Residence District) (66.91 acres) and [C-2](#) (Intermediate Commercial) (2.97 acres)

Proposed Use:

Single-family residential, multifamily residential, and commercial

Location:

Northwest corner of 67th Avenue and Lower Buckeye Road

Owner:

Property Reserve Arizona, LLC

Applicant/Representative:

Sean Lake, Pew & Lake, PLC

Staff Recommendation:

Approval, subject to stipulations

General Plan Conformity			
General Plan Land Use Map Designation		Residential 3.5 to 5 dwelling units per acre and Commercial	
Street Map Classification	Lower Buckeye Road	Arterial	53 to 64-foot north half street
	67th Avenue	Arterial	63 to 33-foot west half street
	71st Avenue	Minor Collector	0-foot east half street

CELEBRATE OUR DIVERSE COMMUNITY AND NEIGHBORHOODS CORE VALUE; DIVERSE NEIGHBORHOODS; LAND USE PRINCIPLE: Include a mix of housing types and densities where appropriate within each village that support a broad range of lifestyles.

The proposed R-2 zoning would allow for single-family residential and multifamily residential uses that support a mix of housing types on the site and is appropriately located along two arterials.

CONNECT PEOPLE AND PLACES CORE VALUES; OPPORTUNITY SITES; LAND USE PRINCIPLE: Support reasonable levels of increased intensity, respectful of local conditions and surrounding neighborhoods.

The proposal will allow development of a vacant site into a single-family and multifamily residential community and commercial development that is compatible with the surrounding area and respectful of local conditions. The proposed development incorporates enhanced landscaping around the perimeter, a multi-use trail, and open space to enhance compatibility with the surrounding area.

BUILD THE SUSTAINABLE DESERT CITY CORE VALUE; TREES AND SHADE; DESIGN PRINCIPLE: Integrate trees and shade into the design of new development and redevelopment projects throughout Phoenix.

As stipulated, the proposal will shade the sidewalk on Lower Buckeye Road, provide enhanced landscaping standards along the perimeter of the development, and provide additional shading within the surface parking lots. This will help to provide shade for pedestrians in and around the community and to mitigate the urban heat island effect by covering hard surfaces, thus cooling the micro-climate around the vicinity.

Applicable Plans, Overlays, and Initiatives

[Estrella Village Plan](#): Background Item No. 6.

[Estrella Village Arterial Street Landscaping Program](#): Background Item No. 7.

[Housing Phoenix Plan](#): Background Item No. 8.

[Monarch Butterfly Pledge](#): Background Item No. 9.

[Tree and Shade Master Plan](#): Background Item No. 10.

[Complete Streets Guidelines](#): Background Item No. 11.

Comprehensive Bicycle Master Plan: Background Item No. 12.

Zero Waste PHX: Background Item No. 13.

Surrounding Land Uses and Zoning		
	<u>Land Use</u>	<u>Zoning</u>
On Site	Vacant	RE-35, R1-8, and C-2
North	Vacant	RE-35
South (across Lower Buckeye Road)	Single-family residential	Maricopa County RU-43
East (across 67th Avenue)	Single-family residential, pharmacy, vacant	R1-8, C-2
West (across 71st Street)	Single-family residential	R1-8

R-2 (Single-family Residence) Planned Residential Development		
<u>Standards</u>	<u>Requirements</u>	<u>Provisions on the Proposed site Plan</u>
Gross Acreage	-	No site plan provided
Total Number of Units	-	No site plan provided
Density (units/gross acre)	6.5; 12 with bonus	No site plan provided
Minimum Lot Width	45 feet	No site plan provided
Subject to Single Family Design Review	Yes	No site plan provided
Open Space	Minimum 5% of gross area	No site plan provided
Maximum Building Height	2 stories and 30 feet	No site plan provided
Building Setbacks		
Street (Front, Rear, Sides)	15 feet (in addition to landscape setbacks)	No site plan provided
Property Line (Sides)	10 feet (1-story) 15 feet (2-story)	No site plan provided
Property Line (Rear)	15 feet (1-story) 20 feet (2-story)	No site plan provided
Lot Coverage	40% not including attached shade structures Total: 50%	No site plan provided

R-2 (Multifamily Residential) (Planned Residential Development Option)		
<u>Standards</u>	<u>Requirements</u>	<u>Provisions on the Proposed site Plan</u>
Gross Acreage	-	52.38 acres
Maximum Number of Units	702, 803 with bonus	330 units (Met)
Maximum Density (dwelling units per acre)	10.50; 12.00 with bonus	6.3 (Met)
Maximum Lot Coverage	45 percent	23 percent (Met)
Building Height/Stories	2 stories and 30 feet for first 150 feet; 1 feet in 5 feet increase to 48 feet high and 4 stories	13 feet within 150 feet of perimeter (Met)
<i>Setbacks</i>		
Adjacent to Streets: South – Lower Buckeye Road	20 feet	20 feet (Met)
Not Adjacent to Streets: North – Adjacent to RE-35	10 feet	30 feet (Met)
Adjacent to Streets: East – Adjacent to 67th Avenue	20 feet	20 feet (Met)
Not Adjacent to Streets: West – Adjacent to R-2	10 feet	20 feet (Met)
<i>Landscaping</i>		
Adjacent to Streets: South – Lower Buckeye Road	20 feet	20 feet (Met)
Not Adjacent to Streets: North – Adjacent to RE-35	5 feet	30 feet (Met)
Adjacent to Streets: East – Adjacent to 67th Avenue	20 feet	20 feet (Met)
Not Adjacent to Streets: West – Adjacent to R-2	5 feet	20 feet (Met)
Minimum Open space	5 percent	16 percent (Met)
Amenities	2 amenities required	8 provided (Met)
Minimum Parking Spaces	2 per unit	1,327 (Met)

*Variance or site plan modification required

C-2 (Intermediate Commercial District)		
<u>Standards</u>	<u>Requirements</u>	<u>Provisions on the proposed site plan</u>
Gross Acreage	-	2.97 acres
<i>MINIMUM BUILDING SETBACKS</i>		
Adjacent to street	Average 25 feet for structures not exceeding two stories or 30 feet, minimum 20 feet permitted for up to 50% of the frontage	No site plan provided
Adjacent to other property lines	When adjacent zoning is S-1, S-2, RE-43 to R-3: 50 feet (2 stories and 30 feet in height)	No site plan provided
<i>MINIMUM LANDSCAPE SETBACKS AND STANDARDS</i>		
Adjacent to street	Average 25 feet for structures not exceeding two stories or 30 feet, minimum 20 feet permitted for up to 50% of the frontage	No site plan provided
Adjacent to other property lines	10 feet	No site plan provided
Maximum Lot Coverage	50%	No site plan provided
Maximum Building Height	2 stories and 30 feet	No site plan provided

Background/Issues/Analysis

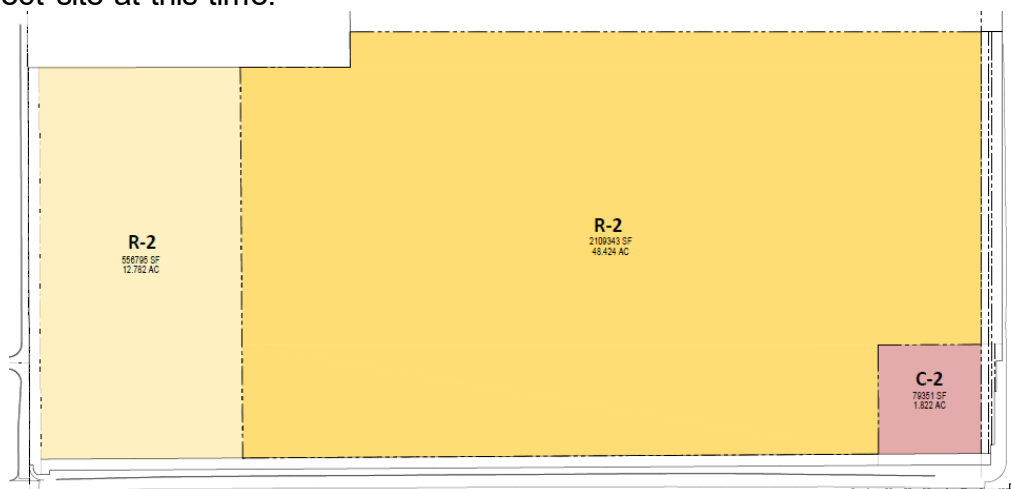
SUBJECT SITE

1. The request is to rezone 69.88 acres located at the northwest corner of 67th Avenue and Lower Buckeye Road from 69 acres of RE-35 (Single-Family Residence District), 0.42 acres of R1-8 (Single-Family Residence District), and 0.46 acres of C-2 (Intermediate Commercial) to 66.91 acres of R-2 (Multifamily Residence District) and 2.97 acres of C-2 (Intermediate Commercial) to allow single-family residential, multifamily residential, and commercial. The subject site is current vacant and undeveloped.

R-2 zoning is proposed on the majority of the subject site. The applicant intends to develop 330 units on the approximate 48-acre portion of the subject site.

R-2 zoning is also proposed on the west portion of the site along 71st Avenue. The applicant intends to develop a single-family residential development on the approximate 12-acre portion of the subject site.

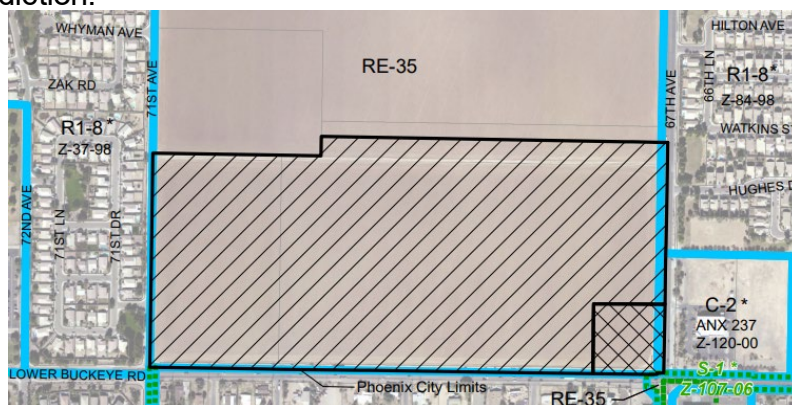
C-2 zoning is proposed on the northeast corner of 67th Avenue and Lower Buckeye Road. There is no defined user for the commercial portion of the subject site at this time.



Zoning Map
Source: Think Architecture

SURROUNDING LAND USES AND ZONING

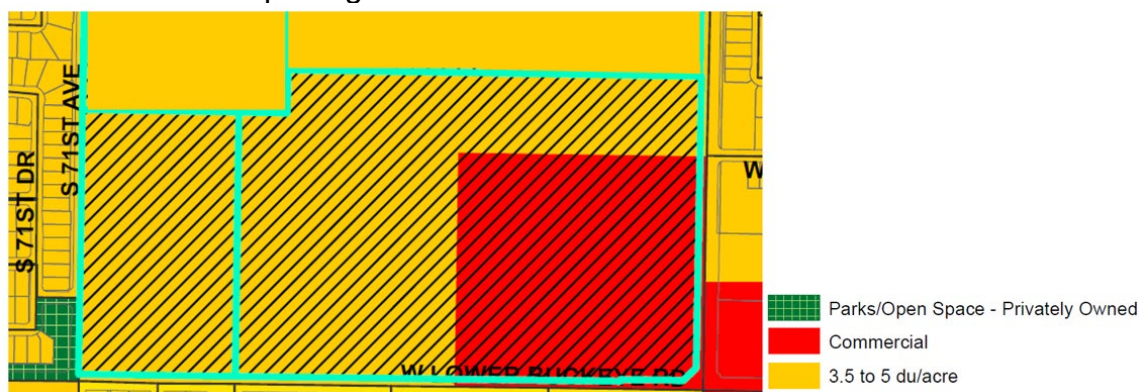
2. The area north of the site is zoned RE-35 and is currently vacant and undeveloped. A single-family development, a pharmacy, and vacant property are located to the east, across 67th Avenue and zoned R1-8 and C-2. To the west, across 71st Avenue, is a single-family residential development zoned R1-8. South, across Lower Buckeye Road, is developed with single-family residential and a church zoned RU-43 (One-Family Residence District) in the Maricopa County jurisdiction.



Existing Zoning Aerial Map
Source: Planning and Development Department

GENERAL PLAN LAND USE MAP DESIGNATION

3. The subject site is designated Residential 3.5 to 5 dwelling units per acre and Commercial on the General Plan Land Use Map. The area to the north is designated Residential 3.5 to 5 dwelling units per acre. To the east, across 67th Avenue, the designation is Residential 3.5 to 5 dwelling units per acre and Commercial. To the south, within Maricopa County, the designation is Residential 2 to 3.5 dwelling units per acre. Furthermore, to the west, across 71st Avenue, the designation is Residential 3.5 to 5 dwelling units per acre and Parks/Open Space – Privately Owned. The R-2 zoning falls within the traditional lot land use category of 3.5 to 5 dwelling units per acre, as a result, a General Plan Amendment is not required. The C-2 zoning is consistent with the General Plan Land Use Map designation of Commercial.



General Plan Land Use Map

Source: City of Phoenix Planning and Development Department

PROPOSAL

4. Site Plan

The concept plan, attached as an exhibit, depicts three separate development areas. The bulk of the site, proposed with R-2 zoning, is depicted as a 330 dwelling unit multifamily development. This development consists of units designed as single-family homes, townhouses, and attached multifamily buildings. A large central open space is provided with numerous amenities including a two-story clubhouse, pool, tennis courts, and tot lot. Furthermore, other amenity areas are proposed on the east portion of the site, along 67th Avenue, and on the south portion of the site along Lower Buckeye Road. In addition, the proposed multifamily development depicts a multi-use trail along Lower Buckeye Road and a large, enhanced entrance with landscaping and pedestrian pathways. In order to ensure the number of units, site configuration, and site improvements, staff is recommending general conformance to the proposed plan. This is addressed in Stipulation No. 24.



Concept Plan
Source: Biltform Architecture Group, Inc.

The west portion of the site, proposed with R-2 zoning, is depicted as an area for future single-family residential subdivision. There is no specific development plan for this R-2 portion of the subject site. However, future development of this approximate 12-acre portion along 71st Avenue will be subject to the approved stipulations for this case and the Zoning Ordinance requirements. Although no development plan exists for this portion today, staff recommends stipulations to require 10 percent open space (Stipulation No. 26) and detached sidewalks (Stipulation No. 9) to make the development compatible with the Village character. Furthermore, staff is recommending a stipulation that would require site plan and building elevations to be presented to the Estrella Village Planning Committee for review and comment prior to preliminary site plan approval. This is addressed in Stipulation No. 23.

There is also no specific development plan for the C-2 portion of the subject site located on the northeast corner of 67th Avenue and Lower Buckeye Road. However, any future development is subject to the approved stipulations for this case and the Zoning Ordinance requirements. Site plans and elevations for any future development on the C-2 portion of the subject site shall be reviewed and approved by the Planning Hearing Officer through the public hearing process. Specific development requirements that will be reviewed by the Planning Hearing Officer include screened refuse bins, accessible pedestrian pathways, and pedestrian connections between adjacent commercial parcels. This is addressed in Stipulation No. 28. To buffer the commercial uses from the adjacent residences, staff recommends Stipulation No. 29 to require evergreen trees be planted along the north and west perimeters. Staff also recommends Stipulation No. 30 to require 6 bicycle parking spaces on site.

5. **Elevations**

The conceptual building elevations for the proposed multifamily development depict architectural features that include variations in texture and color, and pitched roofs. The maximum height within the multifamily development is recommended to be limited to two stories or 35 feet. The proposed height would be compatible with the surrounding single-family residential properties to the east, west, and south.



Conceptual Building Elevations
Source: Barduson Architects

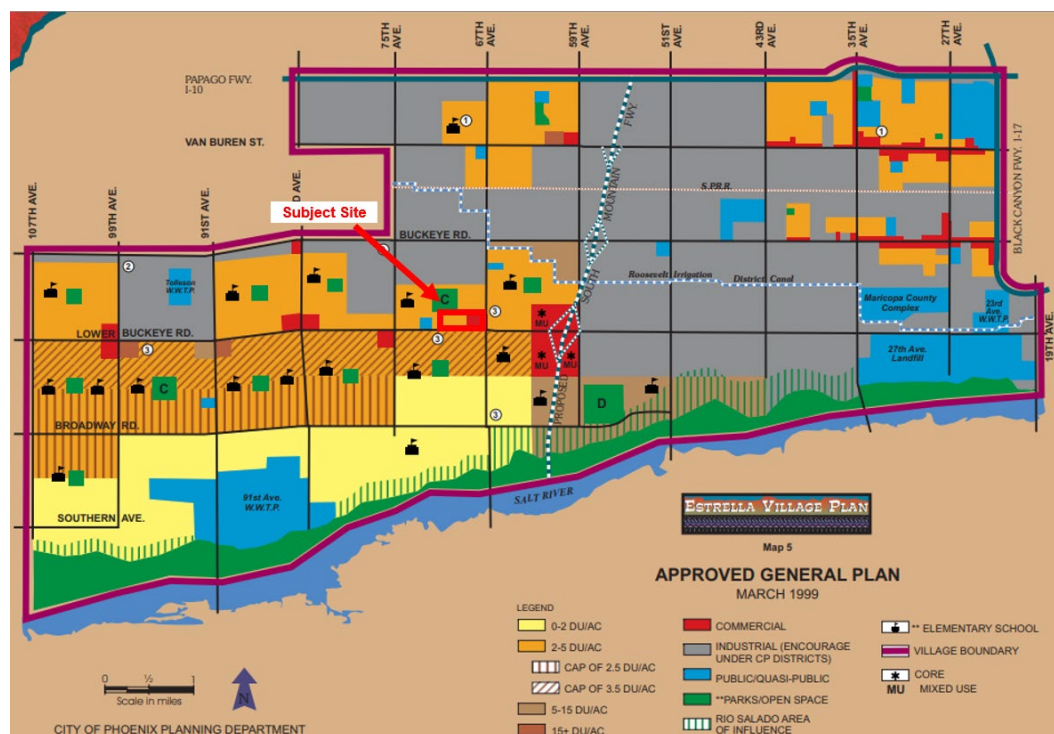
Stipulation No. 1 will ensure that building elevations contain a mix of materials and that all building elevations contain architectural embellishments. This stipulation would ensure enhanced elevations in the proposed multifamily development, and any future development of the R-2 and C-2 portion of the subject site.

STUDIES AND POLICIES

6. **Estrella Village Plan**

The Estrella Village Plan adopted in 1999, outlines a vision for developing the Estrella Village through five main goals that include:

1. Orderly growth;
2. Identifiable village core;
3. Strong residential neighborhoods;
4. Variety of homes and jobs; and
5. Consistent streetscapes and trail linkages.



Estrella Land Use Map
Source: Planning and Development Department

Goal 3 of the Estrella Village Plan, “Protection of residential neighborhoods”, Objective A, “New Residential Development”, recommends that new residential development and retail centers be supported by street circulation systems and surrounding character. Bounded by two arterial streets, the proposed R-2 and C-2 zonings district allows residential and commercial uses that are supported by the existing street network with 67th Avenue and Lower Buckeye Road. The proposed multifamily development consists of units designed as single-family homes, townhomes, and attached multifamily. Additionally, a traditional single-family development is proposed in the future on the west portion of the site.

Goal 4, “Housing and Employment”, Objective B, “Variety of housing densities and types”, recommends a diversity of housing products in the Estrella Village. The proposed R-2 zoning allows for a multifamily residential use on the site. This zoning district will introduce a new land use in the immediate area, allowing for new housing opportunities within the surrounding land area.

Goal 5, “Urban Design”, recommends streetscape and trail systems to be uniform and consistent to provide identity and linkage between neighborhoods and with activity centers. Several stipulations as recommended by staff promote enhanced urban design along the streetscape. Stipulation Nos. 3, 8, 9, and 10 recommend enhanced landscaping and detached sidewalks along 67th Avenue, 71st Avenue, and Lower Buckeye Road. Furthermore, Stipulation No. 4 requires perimeter fencing or walls to include material or textural changes as a decorative

feature. This would enhance the streetscape along the perimeters of the proposed development. In addition, Stipulation No. 11 requires a 10-foot-wide multi-use trail (MUT) along Lower Buckeye Road. The multi-use trail will promote safety and streetscape activity along Lower Buckeye Road. Lastly, Stipulation No. 25 requires an enhanced vehicular entrance constructed with decorative pavers and pedestrian pathways with enhanced landscaping. This enhanced entry will enhance the perimeter to be compatible with the Village character.

7. **Estrella Village Arterial Street Landscaping Program**

The Estrella Village Arterial Street Landscaping Program adopted in 1999, provides a landscape palette for arterial streets within the Estrella Village to help establish the community's character. In addition to providing planning guidelines, the plan also indicates the locations to establish entry gateways that welcome individuals entering the village with an entry sign and an enhanced landscape area of 75 feet by 75 feet in size.

The subject site is adjacent to 67th Avenue and Lower Buckeye Road, arterial streets, and thus staff recommends Stipulation No. 3 to require adherence with the Estrella Village Arterial Street Landscaping Program requirements along the street frontage.

8. **Housing Phoenix**

In June 2020, the Phoenix City Council approved the Housing Phoenix Plan. This Plan contains policy initiatives for the development and preservation of housing with vision of creating a stronger and more vibrant Phoenix through increased housing options for residents at all income levels and family sizes. Phoenix's rapid population growth and housing underproduction has led to a need for over 163,000 new housing units. Current shortages of housing supply relative to demand are a primary reason why housing costs are increasing.

The proposed development supports the Plan's goal of preserving or creating 50,000 housing units by 2030 by contributing to a variety housing types that will address the supply shortage at a more rapid pace while using vacant land in a more sustainable fashion.

9. **Monarch Butterfly Pledge**

In April 2021, Mayor Kate Gallego signed the [National Wildlife Federation's Mayor's Monarch Pledge](#). This pledge commits the city to take action to support the monarch butterfly population. In the United States, loss of milkweed habitat is a major factor in the decline of the monarchs. Arizona has at least 29 species of milkweed native to the state. Adult monarchs feed on the nectar of many flowers, but they breed only where milkweeds are found. To support the monarch butterfly population, Stipulation No. 2 addresses the planting of milkweed shrubs, or other native nectar plant species, on the subject site.

10. **Tree and Shade Master Plan**

The Tree and Shade Master Plan encourages treating the urban forest as infrastructure to ensure the trees are an integral part of the City's planning and development process. Sidewalks on the street frontages should be detached from the curbs to allow trees to be planted on both sides of the sidewalk to provide thermal comfort for pedestrians and to reduce the urban heat island effect. Staff is recommending the following stipulations designed to provide trees and enhance shade within and adjacent to the development:

- Uncovered surface parking lot areas shall be landscaped with minimum two-inch caliper drought-tolerant shade trees to achieve a minimum of 25 percent shade (Stipulation No. 7);
- Detached sidewalks along the perimeter streets with minimum two-inch caliper, single trunk, drought-tolerant trees to provide a minimum of 75 percent shade (Stipulation Nos. 8, 9, and 10);
- Evergreen trees shall be planted within the landscape setback along the north and west perimeter of the C-2 portion of the subject site (Stipulation No. 29);
- The bus stop pad along Lower Buckeye Road shall be shaded to a minimum of 50 percent by vegetation (Stipulation No. 6);

11. **Complete Streets Guidelines**

In 2014, the City of Phoenix City Council adopted the Complete Streets Guiding Principles. The principles are intended to promote improvements that provide an accessible, safe, connected transportation system to include all modes, such as bicycles, pedestrians, transit, and vehicles. To provide a safe pedestrian environment, staff is recommending numerous connections to Lower Buckeye Road, 67th Avenue, and 71st Avenue. These pedestrian walkways would be illuminated with pedestrian scale lighting and utilize enhanced pavement treatments when pedestrian walkways cross vehicular drive aisles. This is addressed in Stipulation No. 5. The development will help pedestrianize the immediate street frontage by providing a detached sidewalk along 67th Avenue, 71st Avenue, and Lower Buckeye Road. This is addressed in Stipulation Nos. 8, 9 and 10. In addition, any street improvements must be done to the City of Phoenix and ADA standards. This is addressed in Stipulation No. 19.

12. **Comprehensive Bicycle Master Plan**

The City of Phoenix adopted the Comprehensive Bicycle Master Plan in 2014 to guide the development of its Bikeway System and supportive infrastructure. The Comprehensive Bicycle Master Plan supports options for both short- and longterm bicycle parking as a means of promoting bicyclist traffic to a variety of

destinations. As stipulated, the project will provide bicycle parking spaces, installed per the requirements in the city's Walkable Urban (WU) Code. Furthermore, a bicycle repair station will be provided and include repair tool such as a tire gauge and pump. These are addressed in Stipulation Nos. 27 and 30.

13. **Zero Waste PHX:**

The City of Phoenix is committed to its waste diversion efforts and has set a goal to become a zero-waste city, as part of the city's overall 2050 Environmental Sustainability Goals. One of the ways Phoenix can achieve this is to improve and expand its recycling and other waste diversion programs. Section 716 of the Phoenix Zoning Ordinance establishes standards to encourage the provision of recycling containers for multifamily, commercial and mixed-use developments meeting certain criteria. The provision of recycling containers was not addressed in the applicant's submittals. However, the city will provide recycling services to the proposed single-family subdivision.

COMMUNITY INPUT SUMMARY

14. As of the writing of this report, staff did not receive any letters in opposition or support from members of the public on this rezoning case.

INTERDEPARTMENTAL COMMENTS

15. The Fire Department indicated there are no problems anticipated with the case and that the site and/or buildings shall comply with the International Fire Code with Phoenix Amendments. Further, the Department commented that they do not know the water supply at this site and noted that fire flow requirements may present a challenge for this site.
16. The Street Transportation Department has requested that the developer submit a Traffic Impact Analysis with a signal warrant analysis at the intersection of 71st Avenue and Lower Buckeye Road. Furthermore, the Street Transportation Department is requiring right-of-way dedications along Lower Buckeye Road, 67th Avenue, and 71st Avenue. In addition, access control for the development should comply with the City of Phoenix and Street Transportation standards and all existing irrigation facilities should be relocated underground outside of the City right-of-way. Finally, all street improvements must include all required elements and meet ADA standards. These requirements are addressed in Stipulation Nos. 13 through 19.
17. The Public Transit Department has indicated that the developer is required to construct two bus stop pads along westbound Lower Buckeye Road. One would be located west of 67th Avenue and another west of the 69th Avenue alignment. This is addressed in Stipulation No. 12.

OTHER

18. The site is located in a larger area identified as being archaeologically

sensitive. If further review by the City of Phoenix Archaeology Office determines the site and immediate area to be archaeologically sensitive, and if no previous archaeological projects have been conducted within this project area, it is recommended that archaeological Phase I data testing of this area be conducted. Phase II archaeological data recovery excavations may be necessary based upon the results of the testing. A qualified archaeologist must make this determination in consultation with the City of Phoenix Archaeologist. In the event archaeological materials are encountered during construction, all ground disturbing activities must cease within a 33-foot radius of the discovery and the City of Phoenix Archaeology Office must be notified immediately and allowed time to properly assess the materials. These are addressed in Stipulation Nos. 20 through 22.

19. Development and use of the site is subject to all applicable codes and ordinances. Zoning approval does not negate other ordinance requirements such as obtaining a use permit to conduct the proposed outdoor use in this zoning district. Other formal actions such as, but not limited to, zoning adjustments and abandonments, may be required.

Findings

1. The proposal will develop a vacant property and as stipulated, will be compatible with the surrounding land uses.
2. The development, as stipulated, will incorporate design and landscaping features that enhance the location, consistent with the Estrella Village Plan.
3. The proposal will allow for additional housing opportunities in the Village and will help alleviate the housing crisis.

Stipulations

OVERALL SITE

1. Building elevations shall be developed to the following standards, as approved by the Planning and Development Department:
 - a. All building elevations shall contain a primary material and a minimum of one secondary material to provide tasteful and balanced enhancements to the architecture. The secondary materials and colors shall be composed of brick, masonry, stone, Hardie Plank, lap siding, shudders, or another exterior accent material that exhibits quality, durability, and consistency with the architectural theme.
 - b. All building elevations shall contain architectural embellishments and

detailing such as textural changes, pilasters, offsets, pop-outs, recesses, variation in window size and location, pitched roofs and/or overhang canopies.

2. A minimum of 10% of the required shrubs, shall be a milkweed or other native nectar species, and shall be planted in groups of three or more, as approved by the Planning and Development Department.
3. The development shall conform with the Estrella Village Arterial Street Landscaping Program landscape palette and landscaping standards along arterial streets in the Estrella Village, except as otherwise noted herein, as approved by the Planning and Development Department.
4. All perimeter walls visible from street right-of-way shall include material and/or textural differences, such as stucco and/or split face block with a decorative element, such as tile or stamped designs, as approved by the Planning and Development Department.
5. A system of pedestrian connections shall be provided as follows, and connecting the following via the most direct route, as approved by the Planning and Development Department:
 - a. A minimum of two pedestrian connections shall be provided from the residential zoned portion of the site to the public sidewalk along Lower Buckeye Road.
 - b. A minimum of one pedestrian connection shall be provided from the multifamily-zoned portion of the site to the public sidewalks along 67th Avenue and 71st Avenue.
 - c. Illuminated pedestrian scale lighting per Section 1304(H)5 for pedestrian pathways connecting the multifamily and commercial development, and along pedestrian entrances to the multifamily development.
 - d. Where pedestrian walkways cross a vehicular path, the pathway shall be constructed of decorative pavers, stamped or colored concrete, or other pavement treatments that visually contrasts parking and drive aisle surfaces.
6. The bus stop pads along Lower Buckeye Road shall be shaded a minimum of 50% by vegetative shade at maturity, as approved by the Planning and Development Department. Where utility conflicts exist, the developer shall work with the Planning and Development Department on alternative design solutions consistent with a pedestrian environment.

7. All uncovered surface parking lot areas shall be landscaped with minimum 2-inch caliper size large canopy drought tolerant shade trees. Landscaping shall be dispersed throughout the parking area and achieve 25% shade at maturity, as approved by Planning and Development Department.
8. The sidewalk along 67th Avenue shall be a minimum of 5 feet in width and detached with a minimum 13-foot-wide landscape strip located between the sidewalk and back of curb. The landscape strip shall be planted with minimum 2-inch caliper, single trunk, drought-tolerant shade trees to provide a minimum of 75% shade at maturity, as approved by the Planning and Development Department. Where utility conflicts exist, the developer shall work with the Planning and Development Department on alternative design solutions consistent with a pedestrian environment.
9. The sidewalk along 71st Avenue shall be a minimum of 5 feet in width and detached with a minimum 8-foot-wide landscape strip located between the sidewalk and back of curb. The landscape strip shall be planted with minimum 2-inch caliper, single trunk, drought-tolerant shade trees to provide a minimum of 75% shade at maturity, as approved by the Planning and Development Department. Where utility conflicts exist, the developer shall work with the Planning and Development Department on alternative design solutions consistent with a pedestrian environment.
10. The sidewalk along Lower Buckeye Road shall be a minimum of 5 feet in width and detached with a minimum 13-foot wide landscape strip located between the sidewalk and back of curb. The landscape strip shall be planted with minimum 2-inch caliper, single trunk, drought-tolerant shade trees to provide a minimum of 75% shade at maturity, as approved by the Planning and Development Department. Where utility conflicts exist, the developer shall work with the Planning and Development Department on alternative design solutions consistent with a pedestrian environment.
11. A 30-foot-wide multi-use trail easement (MUTE) shall be dedicated along Lower Buckeye Road and a minimum 10-foot-wide multi-use trail (MUT) shall be constructed within the easement, in accordance with the MAG supplemental detail and as approved by the Planning and Development Department.
12. The developer shall construct two bus stop pads along westbound Lower Buckeye Road. One west of 67th Avenue and another west of the 69th Avenue alignment. The bus stop pads shall be constructed according to City of Phoenix Standard Detail P1260 with a minimum depth of 10 feet and shall be spaced from the intersection of 67th Avenue and the 69th Avenue alignment according to City of Phoenix Standard Detail P1258.
13. The developer shall dedicate minimum 55-feet of right-of-way and construct the

north side of Lower Buckeye Road, as required by the C-M Cross Section identified on the Street Classification Map and as approved by the Planning and Development Department.

14. The developer shall dedicate minimum 60-feet of right-of-way and construct the west side of 67th Avenue to include a 14-foot-wide landscaped median, as required by the C-M Cross Section identified on the Street Classification Map and as approved by the Planning and Development Department.
15. The developer shall dedicate minimum 40-feet of right-of-way and construct the east side of 71st Avenue, as required by the E Cross Section identified on the Street Classification Map and as approved by the Planning and Development Department.
16. Access control for the development site shall comply with the City of Phoenix Street Planning and Design Guideline Standards and as approved by the Street Transportation Department.
17. The developer shall submit a Traffic Impact Analysis (TIS) to the City for this development. No preliminary approval of plans shall be granted until the study is reviewed and approved by the City. The study shall include signal warrant analysis at the intersection of 71st Avenue and Lower Buckeye Road. The developer shall be responsible for constructing and/or providing funds in escrow all off-site improvements including modifications to existing traffic signals, as recommended by the approved TIS.
18. Existing irrigation facilities along all public streets are to be undergrounded and relocated outside of City right-of-way. Contact SRP to identify existing land rights and establish the appropriate process to relocate the facility. Relocations that require additional dedications or land transfer require completion prior to obtaining plat and/or civil plan review approval.
19. The developer shall construct all streets within and adjacent to the development with paving, curb, gutter, sidewalk, curb ramps, streetlights, median islands, landscaping and other incidentals, as per plans approved by the Planning and Development Department. All improvements shall comply with all ADA accessibility standards.
20. If determined necessary by the Phoenix Archeology Office, the applicant shall conduct Phase I data testing and submit an archaeological survey report of the development area for review and approval by the City Archeologist prior to clearing and grubbing, landscape salvage, and/or grading approval.
21. If Phase I data testing is required, and if, upon review of the results from Phase I data testing, the City Archeologist, in consultation with a qualified archeologist,

determines such data recovery excavations are necessary, the applicant shall conduct Phase II archeological data recovery excavations.

22. In the event archeological materials are encountered during construction, the developer shall immediately cease all ground-disturbing activities within a 33-foot radius of the discovery, notify the City Archeologist, and allow time for the Archeology Office to properly assess the materials.

R-2 Zoned Areas (Both Single-Family and Multifamily)

23. A site plan and building elevations shall be presented to the Estrella Village Planning Committee for review and comment prior to preliminary site plan approval for the area depicted as a single-family residential subdivision on the Concept Plan date stamped November 30, 2022.
24. The approximate 48-acre multifamily development depicted on the Concept Plan date stamped November 30, 2022, shall be in general conformance with the Concept Plan and elevations date stamped November 30, 2022, as modified by the following stipulations and approved by the Planning and Development Department.
25. The primary vehicular entrance to the development shall include the following elements, as approved by the Planning and Development Department:
 - a. Pedestrian pathways connecting the interior of the development to the public sidewalks along both sides of the vehicular driveway.
 - b. The pedestrian pathways shall be detached from the vehicular driveway and lined with landscape areas on both sides of not less than 5 feet in width. The landscape area shall be planted with drought-tolerant plant materials providing seasonal interest and minimum 75% live coverage at maturity.
 - c. A mix of ornamental trees (no less than 2-inch caliper), shrubs (no less than five 5-gallon shrubs per tree) and flower beds that will provide a variety of texture and color throughout the year and minimum 75% live coverage, shall be provided along both sides of the entryway.
 - d. The entry driveway surface shall be constructed of decorative pavers, stamped or colored concrete, or another material, other than those used to pave the parking surfaces.
26. A minimum of 10% of the site area shall be provided as open space for each residential development, exclusive of landscape setbacks, as approved by the Planning and Development Department.

27. The development shall incorporate bicycle infrastructure as described below and as approved by the Planning and Development Department.
 - a. A minimum of 30 bicycle parking spaces shall be provided through Inverted U and/or artistic racks, or in a secure area and located throughout the site including near the centralized open space and installed per the requirements of Section 1307.H. of the Phoenix Zoning Ordinance.
 - b. One bicycle repair station ("fix it station") shall be provided and maintained in the multifamily development portion of the site within a central amenity area or primary site entrance. The bicycle repair station ("fix it station") shall be provided in an area of high visibility and separated from vehicular maneuvering areas, where applicable. The repair station shall include: standard repair tools affixed to the station; a tire gauge and pump affixed to the base of the station or the ground; and a bicycle repair stand which allows pedals and wheels to spin freely while making adjustments to the bike.

C-2 Zoned Area

28. The conceptual site plan and elevations for future development of the commercially zoned area shall be reviewed and approved by the Planning Hearing Officer through the public hearing process for stipulation modifications prior to preliminary site plan approval. This is a legislative review for conceptual purposes only. Specific development standards and requirements will be determined by the Planning Hearing Officer and the Planning and Development Department. The site plan shall incorporate the following elements:
 - a. Refuse bins shall be screened from view by a 6-foot-tall wall.
 - b. Accessible pedestrian pathways that connect building entrances, public sidewalks, and the bus stop pad along Lower Buckeye Road using the most direct route for pedestrians.
 - c. Pedestrian connections between adjacent commercial developments (if developed across multiple phases).
29. Evergreen trees shall be planted within the north and west landscape setbacks, as approved by the Planning and Development Department.
30. A minimum of 6 bicycle parking spaces shall be provided through Inverted U and/or artistic racks installed per the requirements of Section 1307.H. of the Phoenix Zoning Ordinance, as approved by the Planning and Development Department.

Writer

Staff Report: Z-72-21-7

January 13, 2023

Page 20 of 20

Nayeli Sanchez Luna

January 12, 2023

Team Leader

Racelle Escolar

Exhibits

Zoning sketch map

Aerial sketch map

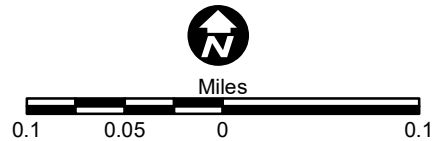
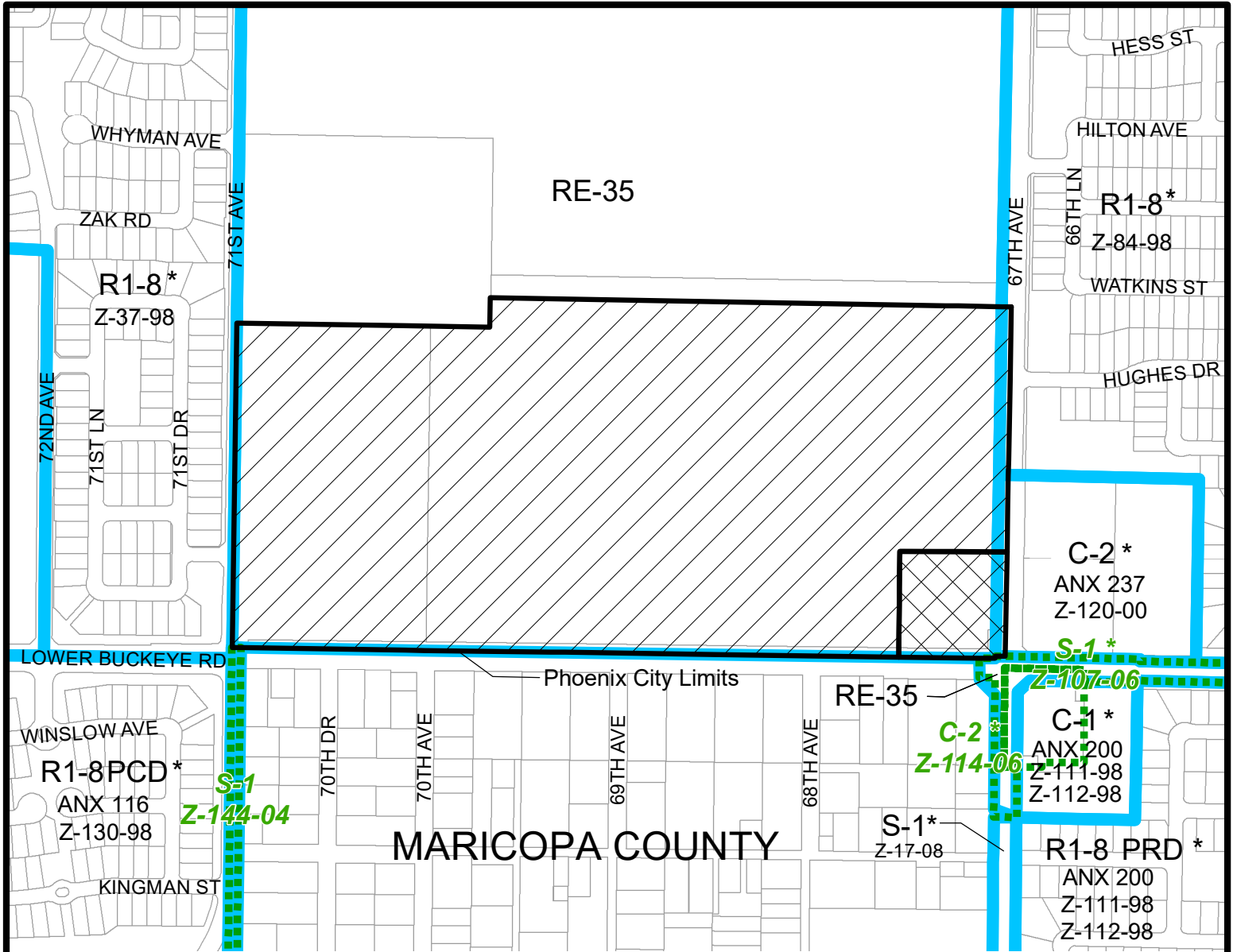
Zoning Plan date stamped November 30, 2022

Concept Plan date stamped November 30, 2022

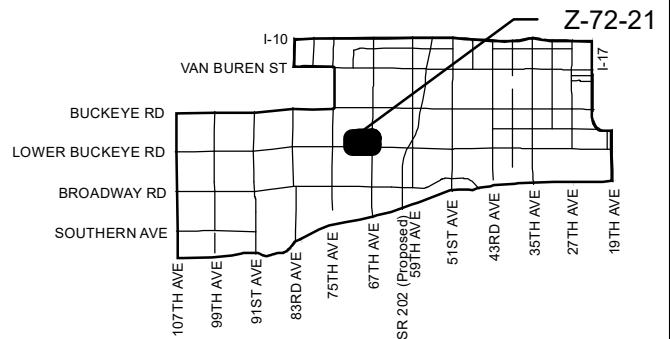
Building Setback Plan date stamped November 30, 2022

Conceptual Building Elevations date stamped November 15, 2021 (8 pages)

Building Renderings date stamped November 15, 2021 (3 pages)



ESTRELLA VILLAGE
CITY COUNCIL DISTRICT: 7



APPLICANT'S NAME: Pew & Lake, PLC - Sean Lake

APPLICATION NO. Z-72-21

GROSS AREA INCLUDING 1/2 STREET
AND ALLEY DEDICATION IS APPROX.

69.88 Acres

DATE: 12/6/2021
REVISION DATES:

12/19/2022 12/23/2022

AERIAL PHOTO &
QUARTER SEC. NO.

QS 7-12

ZONING MAP

E-4

REQUESTED CHANGE:

FROM: RE-35 (69.00 a.c.)
R1-8 (0.42 a.c.)
C-2 (0.46 a.c.)

TO: R-2 (66.91 a.c.) ☒
C-2 (2.97 a.c.) ☒

MULTIPLES PERMITTED

RE-35, R1-8, C-2

R-2, C-2

CONVENTIONAL OPTION

76, 2, 6

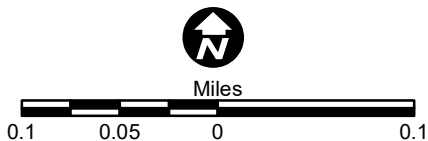
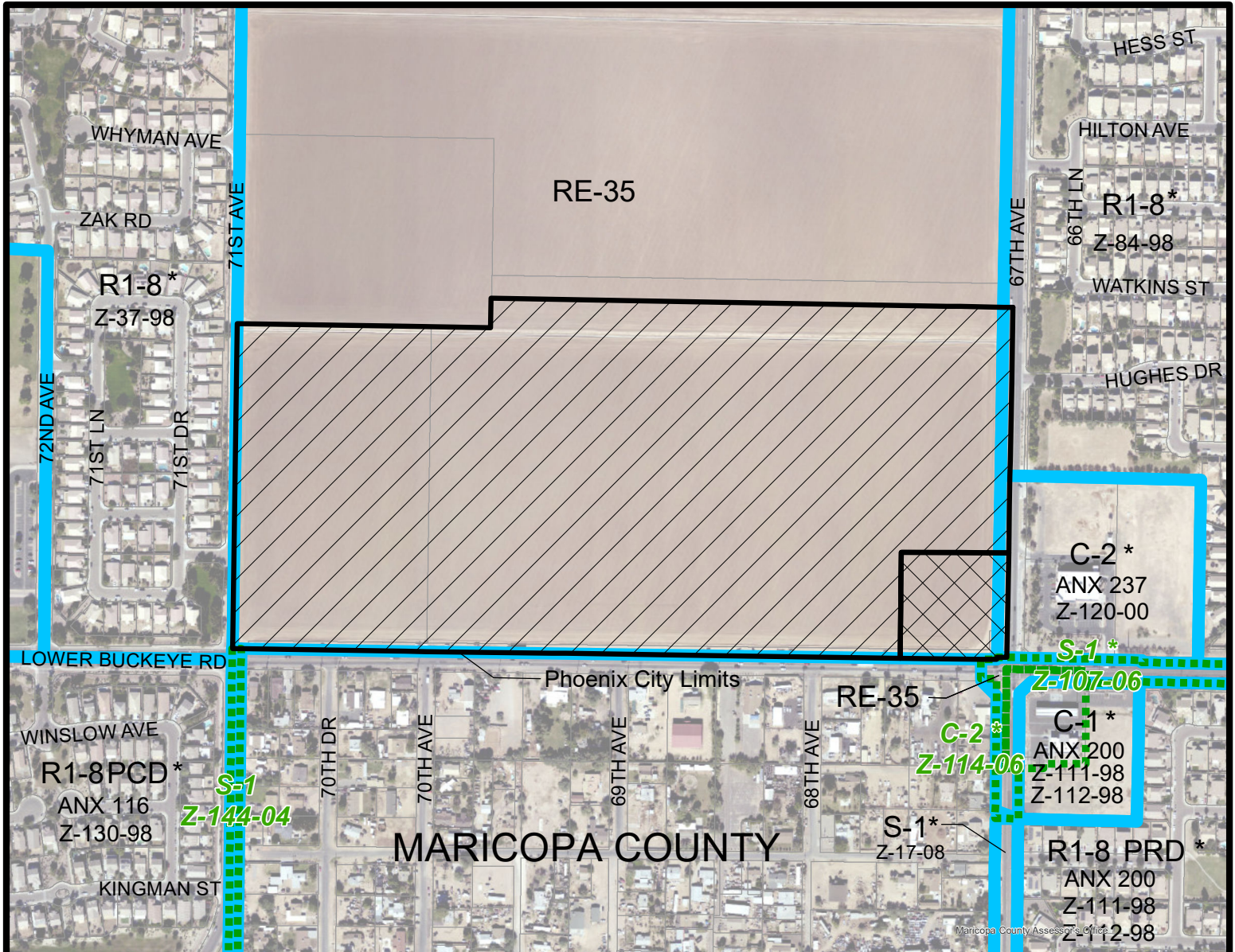
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* UNITS P.R.D. OPTION

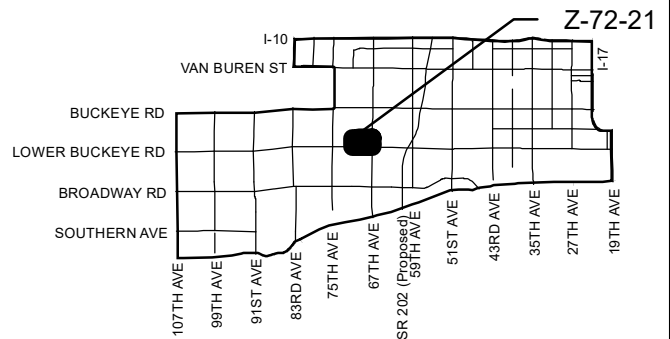
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803, 51

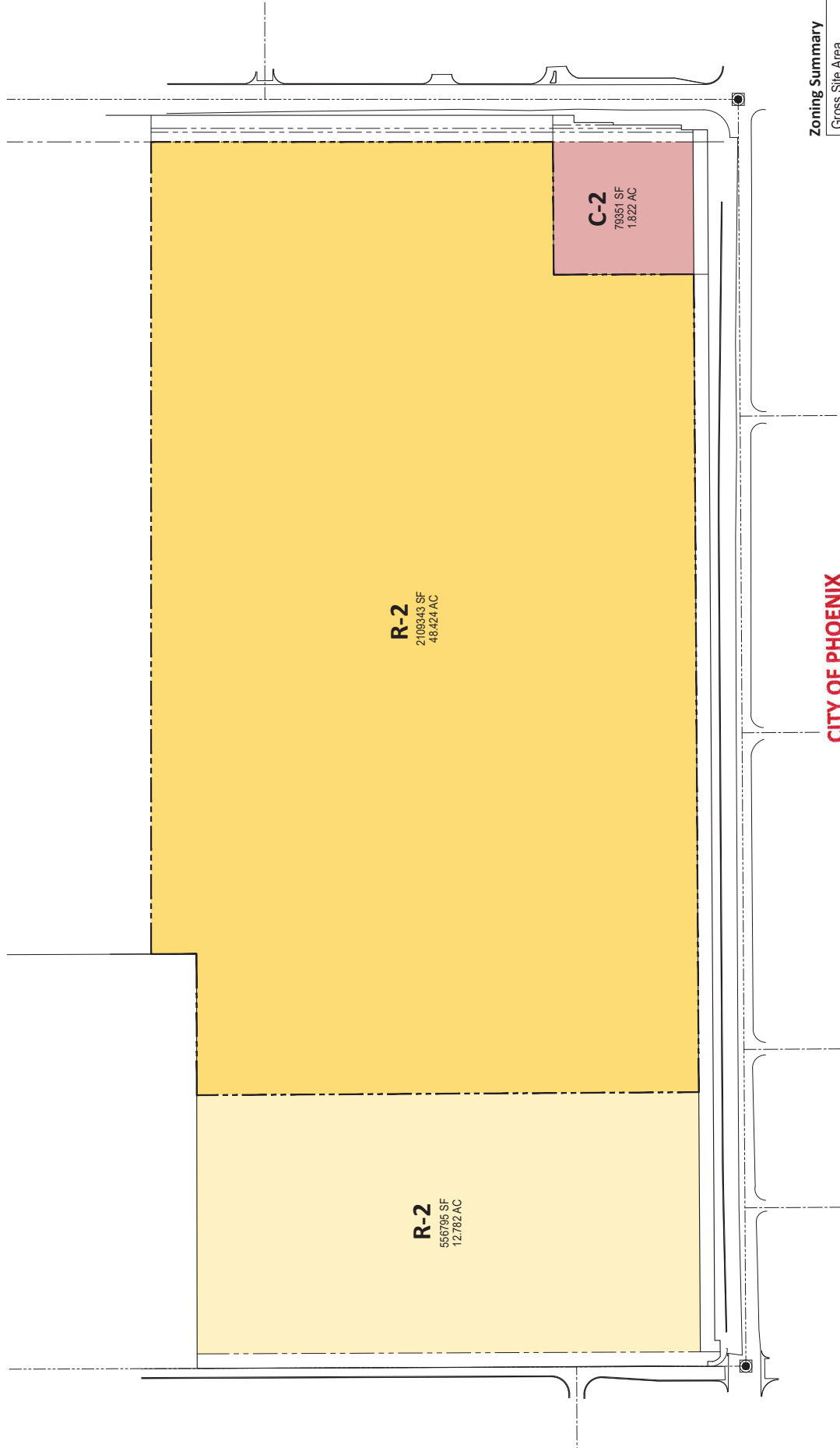
* Maximum Units Allowed with P.R.D. Bonus



ESTRELLA VILLAGE
CITY COUNCIL DISTRICT: 7



APPLICANT'S NAME: Pew & Lake, PLC - Sean Lake		REQUESTED CHANGE:	
APPLICATION NO. Z-72-21	DATE: 12/6/2021 REVISION DATES: 12/19/2022 12/23/2022	FROM: RE-35 (69.00 a.c.) R1-8 (0.42 a.c.) C-2 (0.46 a.c.)	
GROSS AREA INCLUDING 1/2 STREET AND ALLEY DEDICATION IS APPROX. 69.88 Acres	AERIAL PHOTO & QUARTER SEC. NO. QS 7-12	ZONING MAP E-4	TO: R-2 (66.91 a.c.) C-2 (2.97 a.c.)
MULTIPLES PERMITTED RE-35, R1-8, C-2 R-2, C-2	CONVENTIONAL OPTION 76, 2, 6 669, 43		* UNITS P.R.D. OPTION 91, 2, 8 803, 51
* Maximum Units Allowed with P.R.D. Bonus			



R-2
2109343 SF
48.424 AC

R-2
556795 SF
12.782 AC

C-2
79351 SF
1.822 AC

Zoning Summary

Gross Site Area	48.424 Ac
Commercial C-2	1.822 Ac
Residential R-2 PRD	48.424 Ac
Residential R-2	12.782 Ac

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NOV 30 2022
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Department



October 17, 2022



Zoning Plan

Lower Buckeye BFR, Phoenix, AZ



Site Summary

Gross Site Area	67.90 Ac
Net BFR Area	48.424 AC
Total Units	330 Units
Single-family Units	53
Townhome Units	125
Attached Multi-family	152
Net BFR Density	6.81 du/ac
Maximum Bldg Height	35'
Building Cover	22.93%



October 14, 2022



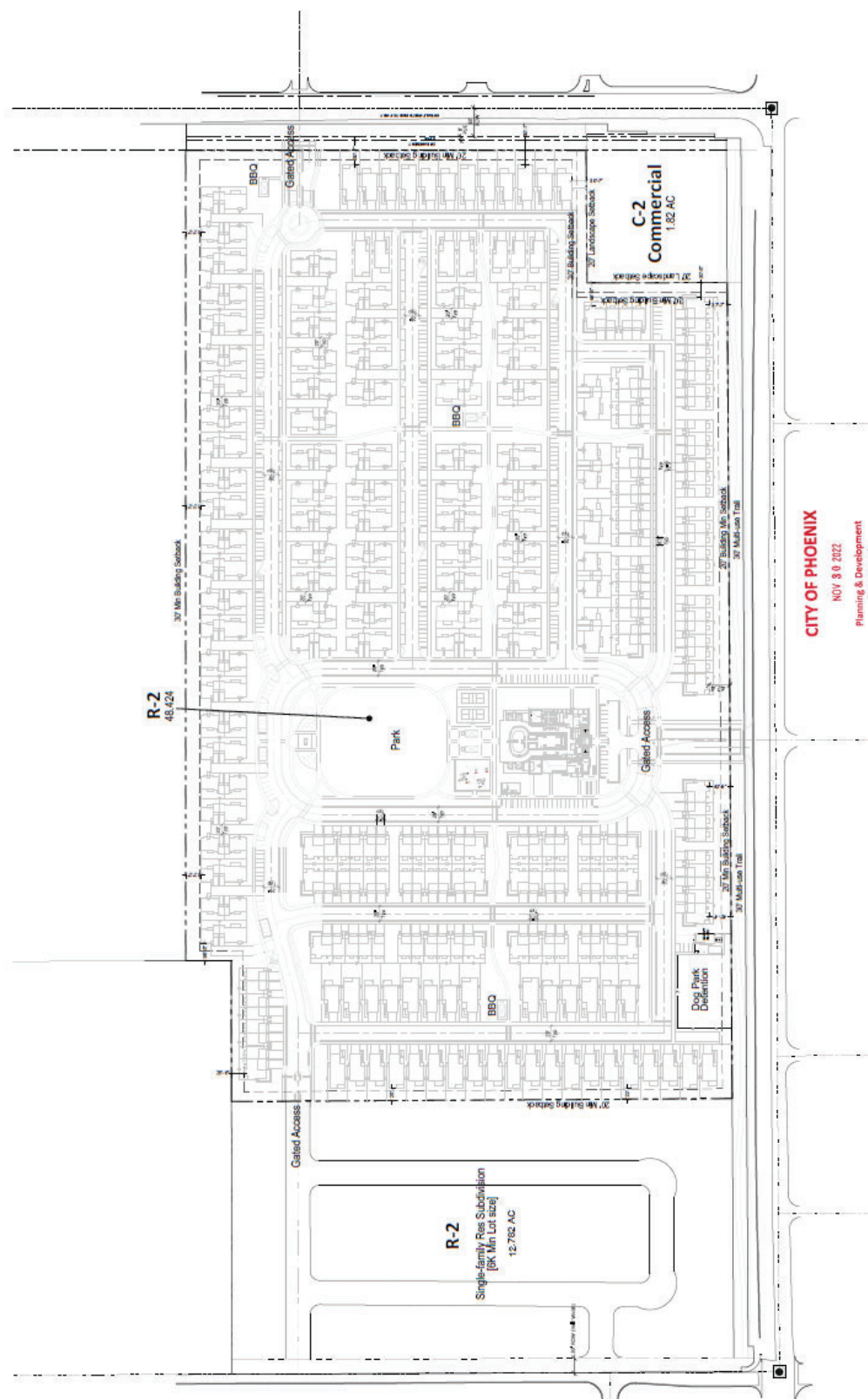
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BFR Concept Plan

Lower Buckeye BFR, Phoenix, AZ



October 14, 2022



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NOV 8 9 2022
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Department

Building Setbacks

Lower Buckeye BFR, Phoenix, AZ

MODERN AGRARIAN

COLOR SCHEME A1

UNIT 1



FRONT



LEFT



RIGHT



REAR

revisions
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PRASADA
SURPRISE

UNIT 1

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project no:

DR2

CITY OF PHOENIX

NOV 15 2021

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Department

UNIT 1 ALL SIDES - MODERN AGRARIAN

MODERN BUNGALOW

COLOR SCHEME B4

UNIT 1



FRONT



LEFT



RIGHT



REAR

revisions

1	
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10	

**PRASADA
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UNIT 1

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UNIT 1 ALL SIDES - MODERN BUNGALOW

MODERN AGRARIAN
COLOR SCHEME A1

UNIT 2



FRONT



LEFT



RIGHT



REAR

revisions

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UNIT 2

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UNIT 2 ALL SIDES - MODERN AGRARIAN

MODERN BUNGALOW

COLOR SCHEME B4

UNIT 2



FRONT



LEFT



RIGHT



REAR

revisions
1
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UNIT 2

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UNIT 2 ALL SIDES - MODERN BUNGALOW

THINK



Planning & Development
Department

1	2	3
---	---	---

UNIT 3 ALL SIDES - MODERN AGRARIAN

MODERN BUNGALOW
COLOR SCHEME B4

UNIT 3



FRONT



LEFT



RIGHT



REAR

revisions

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10	△	

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UNIT 3

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MODERN AGRARIAN
COLOR SCHEME B5

GARAGE
4 BAY W/
3 STORAGE
UNITS



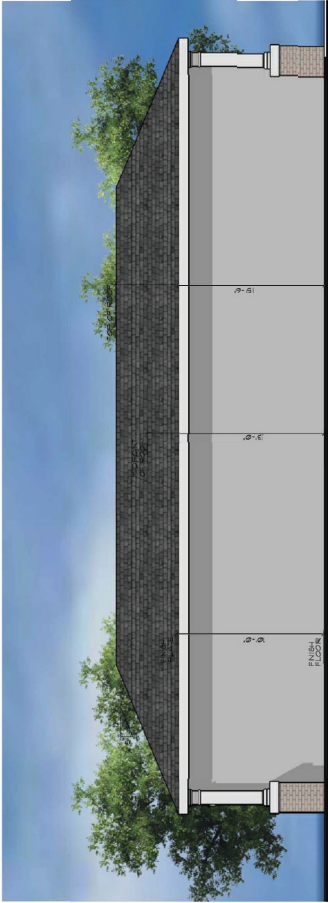
FRONT



LEFT



RIGHT



REAR

revisions	
1	A
2	A
3	A

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SURPRISE

**4 BAY W/
STORAGE**

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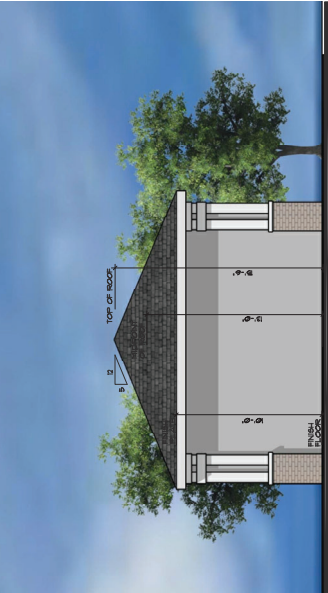
4 BAY GARAGE W/STORAGE - MODERN AGRARIAN

MODERN BUNGALOW
COLOR SCHEME B5

GARAGE
4 BAY W/
3 STORAGE
UNITS



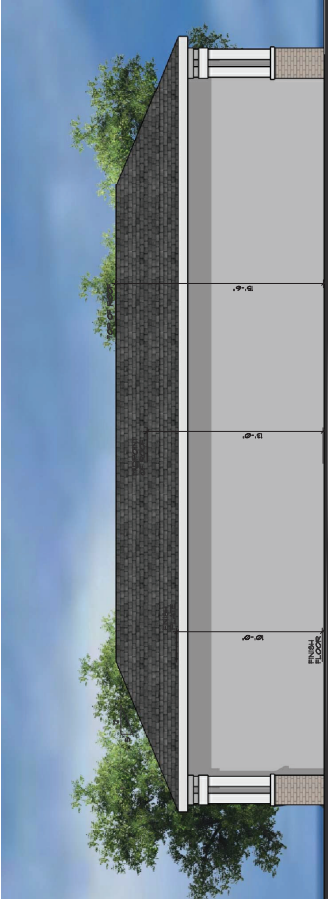
FRONT



LEFT



RIGHT



REAR

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PRASADA
SURPRISE

4 BAY W/
STORAGE

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4 BAY GARAGE W/STORAGE - MODERN BUNGALOW

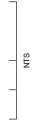




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PERSPECTIVE 1 - SFD CONVENTIONAL
OCTOBER 28, 2020

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NOV 15 2021
Planning & Development
Department

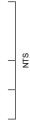
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PERSPECTIVE 2 - 6-PACK CLUSTER
OCTOBER 28, 2020