

## ATTACHMENT B



### City of Phoenix PLANNING AND DEVELOPMENT DEPARTMENT

#### Staff Report Z-104-K-75-3 (Northgate PCD) May 10, 2024

<u>Deer Valley Village Planning Committee</u>	May 21, 2024
Meeting Date:	
<u>Planning Commission</u> Hearing Date:	June 6, 2024
Request From:	<u>PCD</u> (Approved <u>C-2 PCD</u> ) (Planned Community District, Approved Intermediate Commercial, Planned Community District) (3.84 acres)
Request To:	<u>A-1 PCD</u> (Light Industrial, Planned Community District) (3.84 acres)
Proposal:	Industrial
Location:	Approximately 325 feet south of the southeast corner of 21st Avenue and Bell Road
Owner:	Sunbelt Bell 17 LP
Applicant:	Andy Jochums, Gilbert Blilie, PLLC
Representative:	Jeffrey Blilie, Gilbert Blilie PLLC
Staff Recommendation:	Approval, subject to stipulations

<u>General Plan Conformity</u>			
<u>General Plan Land Use Map Designation</u>		Mixed Use (Commercial / Industrial)	
<u>Street Map Classification</u>	21st Avenue	Local	33.58-foot east half street
<b><i>CELEBRATE OUR DIVERSE COMMUNITIES AND NEIGHBORHOODS CORE VALUE; CLEAN NEIGHBORHOODS; LAND USE PRINCIPLE: Facilitate the acquisition of vacant, underutilized and blighted parcels for appropriate redevelopment, compatible with the adjacent neighborhood character and adopted area plans.</i></b>			

The subject site is currently vacant and the proposed industrial development, as stipulated, is compatible with the existing surrounding commercial and industrial character.

**STRENGTHEN OUR LOCAL ECONOMY CORE VALUE;  
MANUFACTURING/INDUSTRIAL DEVELOPMENT; LAND USE PRINCIPLE:  
*Support the expansion of industrial zoning in targeted industrial areas.***

The proposal is compatible with industrial zoned areas to the south and west, and will increase employment opportunities within a Major Employment.

**BUILD THE SUSTAINABLE DESERT CITY CORE VALUE; TREES AND SHADE;  
DESIGN PRINCIPLE: *Integrate trees and shade into the design of new development and redevelopment projects throughout Phoenix.***

The proposal, as stipulated, provides enhanced levels of trees and shade which will reduce the urban heat island effect while also improving thermal comfort to site users and the surrounding neighborhood.

#### **Applicable Plans, Overlays, and Initiatives**

[North I-17 Major Employment Center](#) – See Background Item No. 6.

[Tree and Shade Master Plan](#) – See Background Item No. 7.

[Complete Streets Guiding Principles](#) – See Background Item No. 8.

[Comprehensive Bicycle Master Plan](#) – See Background Item No. 9.

[Zero Waste PHX](#) – See Background Item No. 10.

[Transportation Electrification Action Plan](#) – See Background Item No. 11.

[Community Safety Plan \(CSP\) Areas](#) – See Background Item No. 12.

[Conservation Measures for New Development](#) – See Background Item No. 13.

[Phoenix Climate Action Plan](#) – See Background Item No. 14.

Surrounding Land Uses/Zoning		
	<u>Land Use</u>	<u>Zoning</u>
<b>On Site</b>	Vacant land	PCD (Approved C-2 PCD)
<b>North</b>	Vacant land	PCD (Approved C-2 PCD)
<b>South</b>	Vacant land	PCD (Approved A-1 PCD)
<b>East</b>	Self-service storage warehouse	C-2 SP
<b>West (across 21st Avenue)</b>	Auto sales and repair	C-2 SP PCD

A-1 (Light Industrial)		
<u>Standards</u>	<u>Requirements</u>	<u>Provisions on the Proposed Site Plan</u>
Gross Acreage	-	3.84 acres
Maximum Building Height	56 feet	49 feet (Met)
Maximum Lot Coverage	None	32.74% (Met)
<i>Minimum Building Setbacks</i>		
21st Avenue	0 feet	108 feet (Met)
Interior property lines (north, east, south)	0 Feet	North: 124 feet 5 inches (Met) East: 65 feet 6 inches (Met) South: 0 feet (Met)
<i>Minimum Landscape Setbacks</i>		
21st Avenue	0 feet	Approximately 30 feet (Met)
Interior Property Lines (North, East, South)	0 feet	North: Not depicted East: Not depicted South: 0 feet (Met)
Minimum Parking	344 spaces (for larger development area)	715 spaces (Met)

### **Background/Issues/Analysis**

#### **SUBJECT SITE**

1. This request is to rezone 3.84 acres approximately 325 feet south of the southeast corner of 21st Avenue and Bell Road from PCD (Approved C-2 PCD) (Planned Community District, Approved Intermediate Commercial, Planned Community District) to A-1 PCD (Light Industrial, Planned Community District) to allow industrial uses. The

The subject site is within the Northgate Planned Community District (PCD), an approximately 300-acre master planned community, initially approved by City Council on January 11, 1975, via Rezoning Case No. Z-104-75. The PCD originally covered a land area of 261.8 acres from the I-17 Interstate to 19th Avenue and Bell Road to the Paradise Lane alignment. Several PCD amendments have been completed over the years which increased the total land area of the PCD. As part of this rezoning request, the developer shall update and comply with all applicable master development plans prior to the issuance of the preliminary site plan approval, as approved by the Planning and Development Department. This is addressed in Stipulation No. 1.

2. The General Plan Land Use Map designation for the site is Mixed Use (Commercial / Industrial). This land use category accommodates industrial, office, retail, service and multifamily residential at varying levels of scales and intensity of uses. The proposal for A-1 PCD zoning is consistent with the General Plan Land Use Map designation.

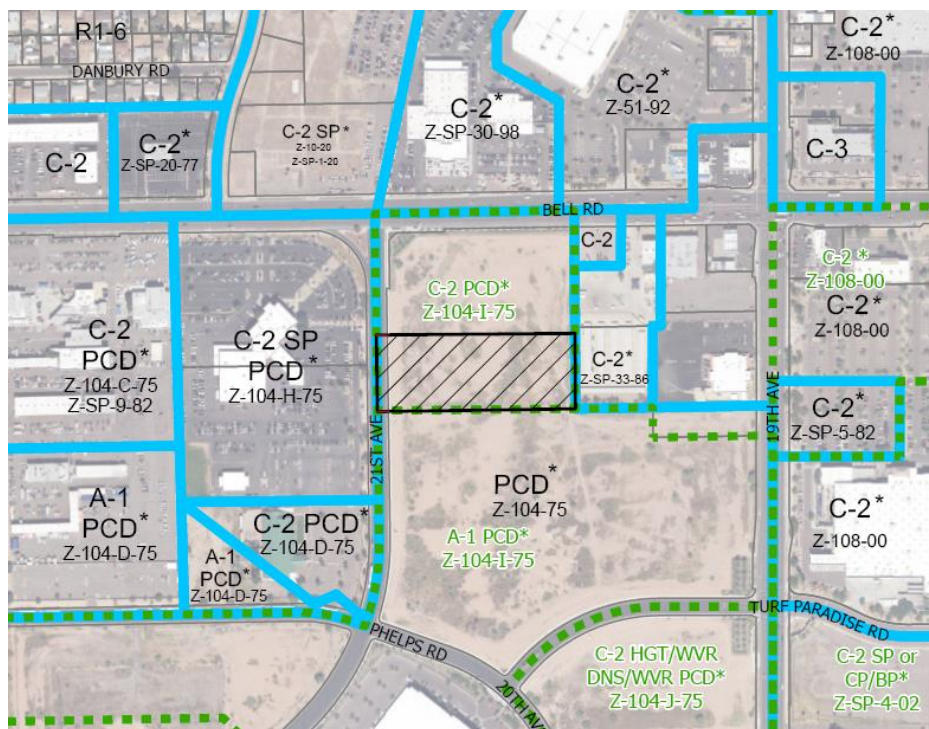
The General Plan Land Use Map designations to the north and south of the subject site are designated as Mixed Use (Commercial / Industrial).



3. The subject site is currently vacant and zoned PCD (Approved C-2 PCD) (Planned Community District, Intermediate Commercial, Planned Community District) (3.84 acres).

To the south of the subject site is vacant zoned PCD (Approved A-1 PCD) (Planned Community District, Light Industrial, Planned Community District).

To the west of the subject site, across 21st Avenue, is a car sales and repair business zoned C-2 SP PCD (Intermediate Commercial, Special Permit, Planned Community District).



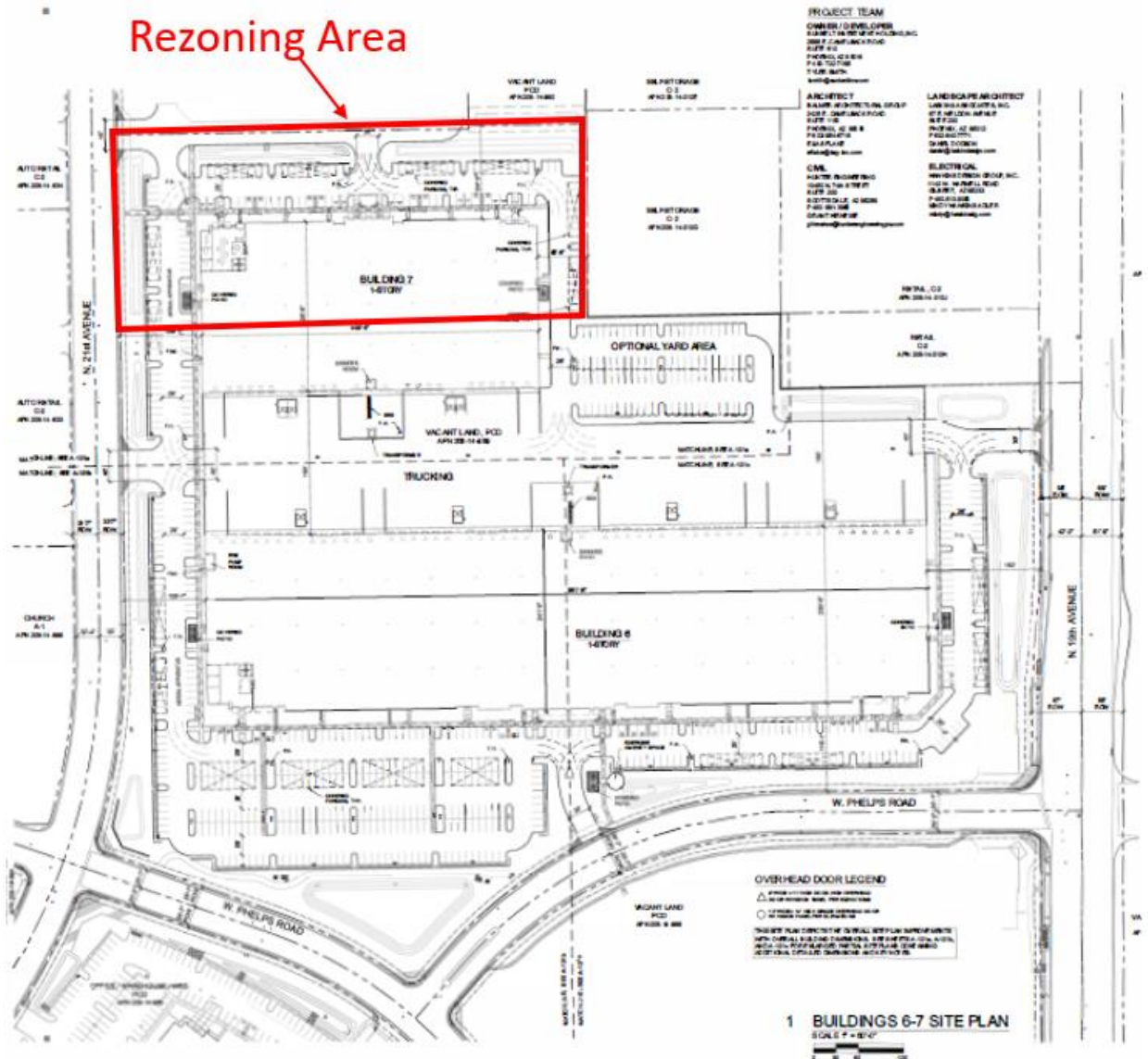
*Zoning Sketch Map*  
*Source: Planning and Development Department*

## 4. Site Plan

The conceptual site plan, attached as an exhibit, proposes two buildings totaling approximately 327,475 square feet to be used for office, warehouse and manufacturing.



The site improvements proposed include four new driveway curb cuts, covered amenity spaces, vehicular parking, trucking/loading space between buildings, surface retention basins, underground retention infrastructure and landscaping. The plan shows 715 vehicle parking spaces.



### Conceptual Site Plan

Source: BALMER architectural group inc.

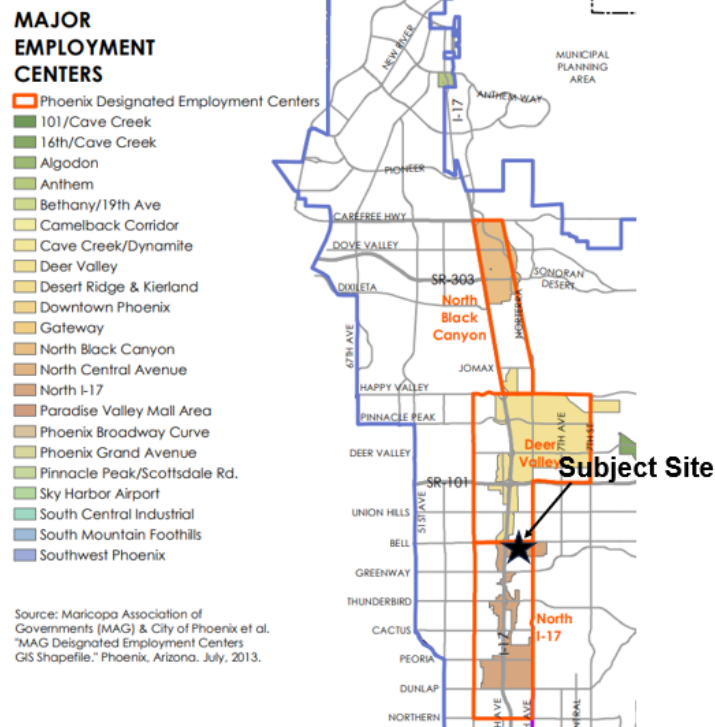
To provide enhanced landscaping, staff recommends a 20-foot landscape setback along 21st Avenue with greater caliper tree sizes. Additionally, staff recommends that surface parking areas be shaded to 25 percent. This is addressed in Stipulation Nos. 16 and 26.

To allow for a shaded area for employees to rest, staff recommends Stipulation No. 19.

The building elevations, attached as an exhibit, depict one-story buildings with a height of 47 feet and 49 feet.



The proposed project is situated within the North I-17 Major Employment Center which is home to a large labor pool within easy access to the regional freeway system. The substantial size and diverse nature of the labor pool found in the North Interstate-17 corridor is a key factor in contributing to the overall attractiveness of this employment center. This proposal adds industrial uses that will add to the employment in a designated employment center.



*Major Employment Centers Map*  
*Source: Planning and Development Department*

7. **Tree and Shade Master Plan**

The Tree and Shade Master Plan has a goal of treating the urban forest as infrastructure to ensure that trees are an integral part of the city's planning and development process. By investing in trees and the urban forest, the city can reduce its carbon footprint, decrease energy costs, reduce storm water runoff, increase biodiversity, address the urban heat island effect, clean the air, and increase property values. In addition, trees can help to create walkable streets and vibrant pedestrian places. Staff recommends enhanced landscape and planting standards adjacent to 21st Avenue, and that a shaded employee resting area be provided, and that surface parking areas be shaded to 25 percent. These are addressed in Stipulation Nos. 16, 19, and 26.

8. **Complete Streets Guiding Principles**

In 2014, the City of Phoenix City Council adopted the Complete Streets Guiding Principles. The principles are intended to promote improvements that provide an accessible, safe, connected transportation system to include all modes, such as bicycles, pedestrians, transit, and vehicles.

Staff recommends detached sidewalks on perimeter streets which shall comply with all applicable master plans. These are addressed in Stipulation Nos. 1 through 6.



Additionally, Stipulation Nos. 14 and 15 will ensure that street improvements will be compliant with city and ADA accessibility standards.

9. **Comprehensive Bicycle Master Plan**

The City of Phoenix adopted the Comprehensive Bicycle Master Plan in 2014 to guide the development of its Bikeway System and supportive infrastructure. The Comprehensive Bicycle Master Plan supports options for both short- and long-term bicycle parking as a means of promoting bicyclist traffic to a variety of destinations. As stipulated, the development will provide bicycle parking and electrical receptacles for electric bicycle charging capabilities which are addressed in Stipulations Nos. 20 and 21.

10. **Zero Waste PHX**

The City of Phoenix is committed to its waste diversion efforts and has set a goal to become a zero-waste city, as part of the city's overall 2050 Environmental Sustainability Goals. One of the ways Phoenix can achieve this is to improve and expand its recycling and other waste diversion programs. Section 716 of the Phoenix Zoning Ordinance establishes standards to encourage the provision of recycling containers for multifamily, commercial, and mixed-use developments meeting certain criteria. The applicant's submittal materials indicated that recycling will be encouraged, and recycling containers provided.

11. **Transportation Electrification Action Plan**

In June 2022, the Phoenix City Council approved the Transportation Electrification Action Plan. The current market desire for the electrification of transportation is both a national and global phenomenon, fueled by a desire for better air quality, a reduction in carbon emissions, and a reduction in vehicle operating and maintenance costs. Businesses, governments and the public are signaling strong future demand for electric vehicles (EVs), and many automobile manufacturers have declared plans for a transition to fully electric offerings within the coming decade. This Plan contains policy initiatives to prepare the City for a future filled with more EVs, charging infrastructure and e-mobility equity, and outlines a roadmap for a five-step plan to prepare for the EV infrastructure needs of 280,000 EVs in Phoenix by 2030. One goal of the Plan to accelerate public adoption of electric vehicles through workplace, business, and multifamily charging infrastructure recommends a standard stipulation for rezoning cases to provide EV charging infrastructure. Staff recommends that a minimum of five percent of the required parking spaces include EV Installed infrastructure and a minimum of five percent of the required parking spaces include EV Capable infrastructure. This is addressed in Stipulation No. 22. Also, bicycle charging capabilities shall be provided through standard electrical receptacles per Stipulation No. 21.

12. **Community Safety Plan (CSP) Areas**

In 2022, the City Council approved two pilot projects to address community safety through a multidisciplinary, collaborative manner leveraging technology as well as community-based resources. The Neighborhood Services, Police, Street Transportation, and Parks and Recreation departments, along with the Office of Homeless Solutions, Public Health Adviser, and Prosecutor's Office, are working together to improve the safety and quality of life along 19th Avenue, 27th Avenue, Hatcher Road, and the I-17 freeway. To support these efforts, the Planning and Development Department promotes Crime Prevention Through Environmental Design (CPTED) principles to enhance neighborhood safety in these Community Safety Plan areas. The subject site is located adjacent to a Community Safety Plan (CSP) area; thus, staff recommends site lighting be provided at the building entrances/exits, and in public assembly and parking areas as addressed in Stipulation No. 17.

13. **Conservation Measures for New Development**

In June 2023, the Phoenix City Council adopted the Conservation Measures for New Development policy as part of a resolution addressing the future water consumption of new development (Resolution 22129). This resolution addresses the future water consumption of new development to support one of the City's Five Core Values in the General Plan which calls for Phoenix to - Build the Sustainable Desert City. The Conservation Measures for New Development policy includes direction to develop standards for consideration as stipulations for all rezoning cases that will address best practices related to water usage in nine specific categories. This is addressed in Stipulation Nos. 23 through 27.

14. **Phoenix Climate Action Plan**

In October 2021, the Phoenix City Council approved the Climate Action Plan. The Climate Action Plan will serve as a long-term plan to achieve greenhouse gas emissions reductions and resiliency goals from local operations and community activities as well as prepare for the impacts of climate change. This plan contains policy and initiatives regarding stationary energy, transportation, waste management, air quality, local food systems, heat, and water. Goal W2 (Water), Action W2.4, pertains to the implementation of the Greater Phoenix Green Infrastructure (GI) and Low Impact Development Details for Alternative Stormwater Management to benefit the environment, promote water conservation, reduce urban heat, improve the public health, and create additional green spaces. This goal is addressed in Stipulation No. 27, which requires a minimum of one GI technique for stormwater management to be implemented in the development.

COMMUNITY INPUT SUMMARY

15. At the time this staff report was written, staff received no correspondence related to this rezoning request.

INTERDEPARTMENTAL COMMENTS

16. **Street Transportation Department**

The Street Transportation Department requested the following:

- A minimum six-foot-wide detached sidewalk separated by a minimum 10-foot-wide landscape strip located between the back of curb and sidewalk be constructed on the west side of 19th Avenue, adjacent to the development.
- A minimum five-foot-wide detached sidewalk separated by a minimum five-foot-wide landscape strip located between curb and sidewalk shall be constructed on the north side of Grandview Road, adjacent to the development.
- A minimum five-foot-wide detached sidewalk separated by a minimum five-foot-wide landscape strip located between curb and sidewalk shall be constructed on the south side of Phelps Road, adjacent to the development.
- A minimum five-foot-wide detached sidewalk separated by a minimum five-foot-wide landscape strip located between the back of curb and sidewalk shall be constructed on the west side of 20th Avenue, adjacent to the development.
- Sidewalk and landscaping improvements within the business park, not adjacent to the development, shall be constructed as approved by the Bell 17 Business Park TIA and Master Phasing Plan.
- A minimum 40-feet of right-of-way shall be dedicated for the north side of Grandview Road from 19th Avenue to 23rd Avenue.
- A minimum 65-feet of right-of-way shall be dedicated for the south side of Bell Road.
- The intersection at 21<sup>st</sup> Avenue and Grandview Road shall be reconstructed to remove the splitter islands.
- A traffic signal shall be fully funded and installed at the intersection for 19th Avenue and Grandview Road.
- The intersection at 19th Avenue and Grandview Road shall be reconstructed, including removal of the splitter island.
- The existing traffic signal at the Phelps Road and 19th Avenue intersection shall be fully funded and reconstructed to meet City of Phoenix standards, as approved by the Street Transportation Department.
- A traffic signal shall be fully funded and installed at the intersection 21<sup>st</sup> Avenue and Bell Road and all associated improvements to the western 21<sup>st</sup> Ave and Bell traffic signal. The intersection shall be reconstructed to meet City of Phoenix standards, as approved by the Street Transportation Department.
- Replace unused driveways with sidewalk and curb and gutter. Also, replace any broken or out-of-grade curb, gutter, sidewalk, curb ramps on all streets and upgrade all off-site improvements to be in compliance with current ADA guidelines.
- All streets be constructed with all required elements and to ADA requirements.

These are addressed in Stipulation Nos. 2 through 15.

**OTHER**

17. The site has not been identified as being archaeologically sensitive. However, in the event archaeological materials are encountered during construction, all ground disturbing activities must cease within 33-feet of the discovery and the City of Phoenix Archaeology Office must be notified immediately and allowed time to properly assess the materials. This is addressed in Stipulation No. 28.
18. Staff has not received a completed form for the Waiver of Claims for Diminution in Value of Property under Proposition 207 (A.R.S. 12-1131 et seq.), as required by the rezoning application process. Therefore, a stipulation has been added to require the form be completed and submitted prior to preliminary site plan approval. This is addressed in Stipulation No. 29.
19. Development and use of the site is subject to all applicable codes and ordinances. Zoning approval does not negate other ordinance requirements. Other formal actions such as, but not limited to, zoning adjustments and abandonments may be required.

**Findings**

1. The request is consistent with the General Plan Land Use Map designation and with several General Plan principles.
2. The proposal will provide increased employment opportunities in the area and is located within the North I-17 Employment Center.
3. The proposed development will improve and enhance a long vacant and underutilized site, which will contribute to enhancing the area.

**Stipulations**

1. The developer shall update and comply with all applicable master development plans prior to the issuance of the preliminary site plan approval, as approved by the Planning and Development Department.
2. A minimum 6-foot-wide detached sidewalk and a minimum 10-foot-wide landscape area between back of curb and sidewalk shall be constructed on the west side of 19<sup>th</sup> Avenue, adjacent to the development.
3. A minimum 5-foot-wide detached sidewalk and a minimum 5-foot-wide landscape strip located between curb and sidewalk shall be constructed on the north side of Grandview Road, adjacent to the development.

4. A minimum 5-foot-wide detached sidewalk and a minimum 5-foot-wide landscape strip located between curb and sidewalk on the south side of Phelps Road, adjacent to the development.
5. A minimum 5-foot-wide detached sidewalk and a minimum 5-foot-wide landscape strip located between the back of curb and sidewalk shall be constructed on the west side of 20<sup>th</sup> Avenue, adjacent to the development.
6. Sidewalk and landscaping improvements within the business park, not adjacent to the development, shall be constructed as approved by the Bell 17 Business Park TIA and Master Phasing Plan.
7. A minimum 40-feet of right-of-way shall be dedicated for the north side of Grandview Road from 19<sup>th</sup> Avenue to 23<sup>rd</sup> Avenue.
8. A minimum 65-feet of right-of-way shall be dedicated for the south side of Bell Road.
9. The intersection at 21<sup>st</sup> Avenue and Grandview Road shall be reconstructed to remove the splitter islands.
10. A traffic signal shall be fully funded and installed at the intersection for 19th Avenue and Grandview Road.
11. The intersection at 19th Avenue and Grandview Road shall be reconstructed, including removal of the splitter island.
12. The existing traffic signal at the Phelps Road and 19th Avenue intersection shall be fully funded and reconstructed to meet City of Phoenix standards, as approved by the Street Transportation Department.
13. A traffic signal shall be fully funded and installed at the intersection 21st Avenue and Bell Road and all associated improvements to the western 21<sup>st</sup> Ave and Bell traffic signal. The intersection shall be reconstructed to meet City of Phoenix standards, as approved by the Street Transportation Department.
14. Replace unused driveways with sidewalk and curb and gutter. Also, replace any broken or out-of-grade curb, gutter, sidewalk, curb ramps on all streets and upgrade all off-site improvements to be in compliance with current ADA guidelines.
15. All streets within and adjacent to the development shall be constructed with paving, curb, gutter, sidewalk, curb ramps, streetlights, median islands, landscaping and other incidentals as per plans approved by the Planning and Development Department. All improvements shall comply with all ADA accessibility standards.



16. A minimum 20-foot landscape setback shall be provided along 21st Avenue, planted with minimum 2-inch caliper, drought-tolerant shade trees, as approved by the Planning and Development Department.
17. Site lighting shall be provided at building entrances/exits, and in public assembly and parking areas, as approved by the Planning and Development Department.
18. Where pedestrian walkways cross a vehicular path, the pathway shall be constructed of decorative pavers, stamped or colored concrete, or other pavement treatments that visually contrasts parking and drive aisle surfaces, as approved by the Planning and Development Department.
19. One outdoor employee resting area of no less than 400-square feet or two 200-square foot areas shall be provided on site. Each required pedestrian area shall include a minimum of two pedestrian seating benches, constructed of quality and durable materials, and shaded to a minimum of 75% using minimum 2-inch caliper large canopy drought-tolerant shade trees and/or architectural shade, as approved by the Planning and Development Department.
20. Bicycle parking shall be provided per the requirements of Section 1307.H. through Inverted U and/or artistic racks and installed per the requirements of Section 1307.H. of the Phoenix Zoning Ordinance. Artistic racks shall adhere to the City of Phoenix Preferred Designs in Appendix K of the Comprehensive Bicycle Master Plan.
21. A minimum of 10 percent of the provided bicycle parking spaces shall include standard electrical receptacles for electric bicycle charging capabilities, as approved by the Planning and Development Department.
22. A minimum of 5 percent of the required parking shall be EV Installed and 5 percent of the required parking shall be EV Capable.
23. Only landscape materials listed in the Phoenix Active Management Area Low-Water-Use/Drought-Tolerant Plan List shall be utilized, as approved or modified by the Planning and Development Department.
24. Landscaping shall be maintained by permanent and automatic/water efficient WaterSense labeled irrigation controllers (or similar smart controller) to minimize maintenance and irrigation water consumption for all on and offsite landscape irrigation.
25. Pressure regulating sprinkler heads and drip lines shall be utilized in any turf areas to reduce water waste.

26. A minimum of 25% of the surface parking areas shall be shaded, as approved by the Planning and Development Department. Shade may be achieved by structures or by minimum 2-inch caliper, drought tolerant, shade trees at maturity, or a combination thereof.
27. A minimum of one green infrastructure (GI) technique for stormwater management shall be implemented per the Greater Phoenix Metro Green Infrastructure and Low-Impact Development Details for Alternative Stormwater Management, as approved or modified by the Planning and Development Department.
28. In the event archaeological materials are encountered during construction, the developer shall immediately cease all ground-disturbing activities within a 33-foot radius of the discovery, notify the City Archaeologist, and allow time for the Archaeology Office to properly assess the materials.
29. Prior to final site plan approval, the landowner shall execute a Proposition 207 waiver of claims form. The waiver shall be recorded with the Maricopa County Recorder's Office and delivered to the City to be included in the rezoning application file for record.

**Writer**

Matteo Moric

May 10, 2024

**Team Leader**

Racelle Escolar

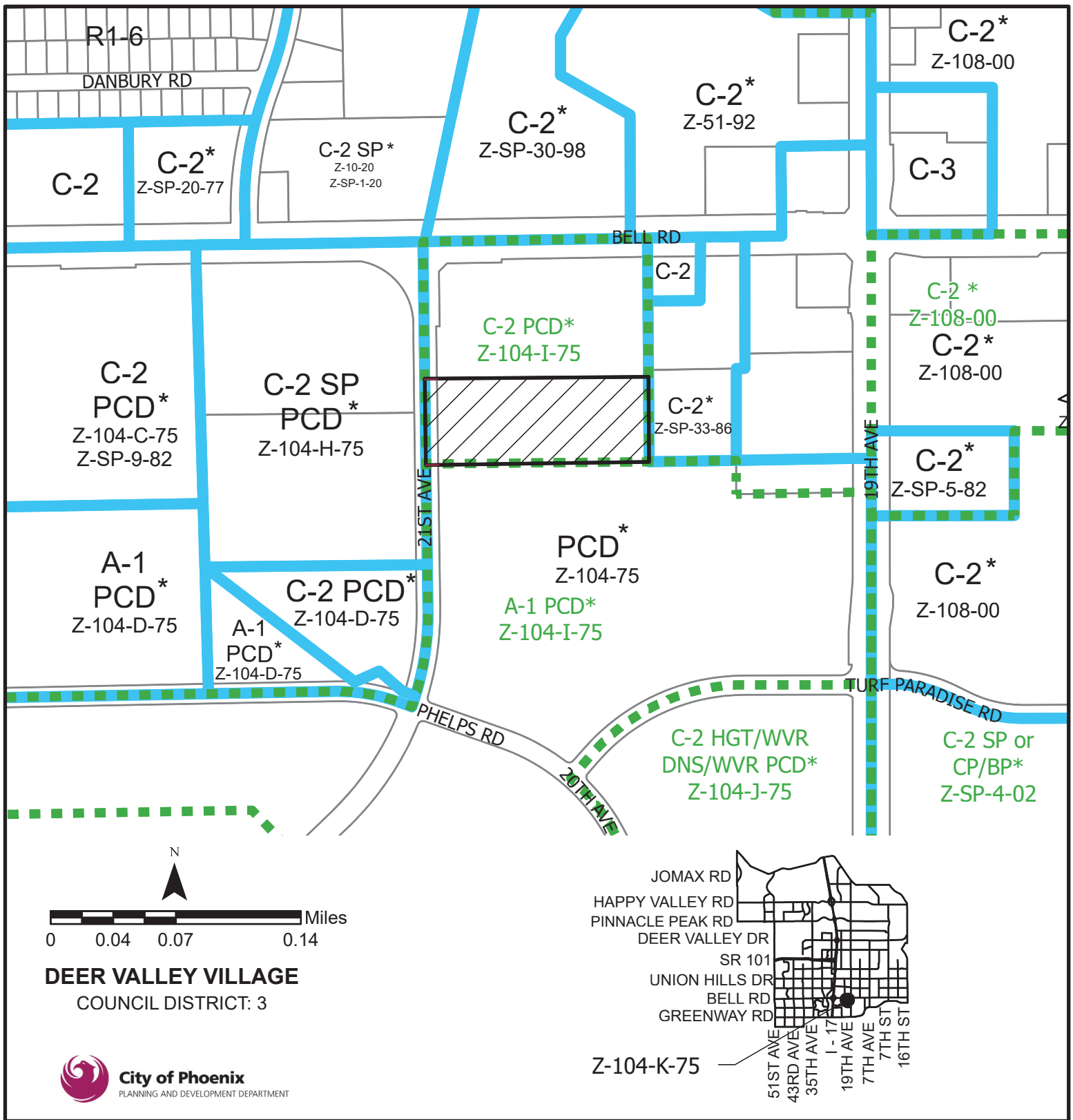
**Exhibits**

Zoning Sketch Map

Aerial Sketch Map

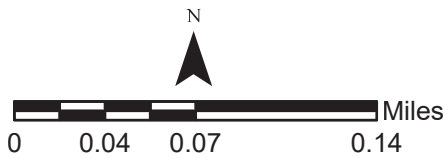
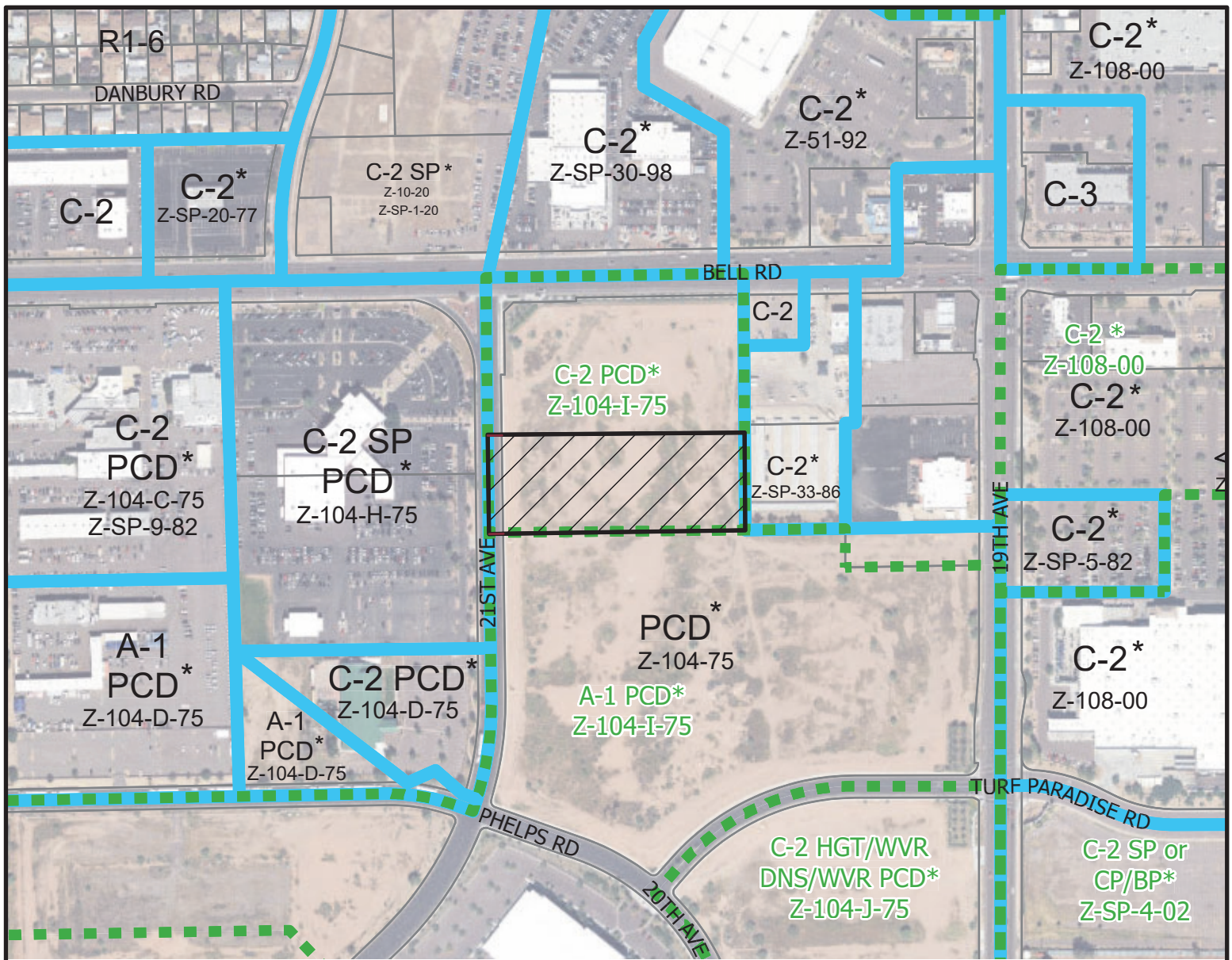
Conceptual Site Plan date stamped March 8, 2024

Conceptual Elevations date stamped March 8, 2024 (2 pages)

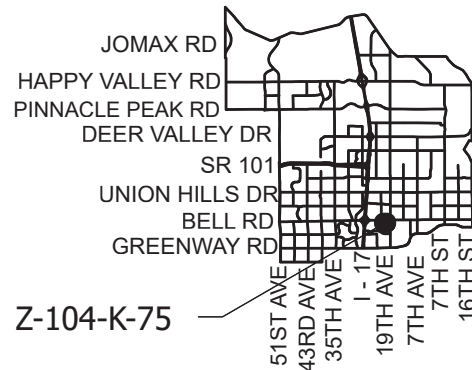


APPLICANT'S NAME: <b>Gilbert Blilie, PLLC</b>		REQUESTED CHANGE:	
APPLICATION NO: <b>Z-104-K-75-3</b>		FROM: <b>PCD (Approved C-2 PCD) ( 3.84 a.c.)</b>	
DATE: <b>3/27/2024</b>		TO: <b>A-1 PCD ( 3.84 a.c.)</b>	
GROSS AREA INCLUDING 1/2 STREET AND ALLEY DEDICATION IS APPROX.		REVISION DATES:	
<b>3.84 Acres</b>			
AERIAL PHOTO & QUARTER SEC. NO.		ZONING MAP	
<b>QS 36-24</b>		<b>M-7</b>	
MULTIPLES PERMITTED		CONVENTIONAL OPTION	
<b>PCD (Approved C-2 PCD)</b>		<b>N/A (55)</b>	
<b>A-1 PCD</b>		<b>N/A</b>	
		* UNITS P.R.D OPTION	
		<b>N/A (67)</b>	
		<b>N/A</b>	

\* Maximum Units Allowed with P.R.D. Bonus



**DEER VALLEY VILLAGE**  
COUNCIL DISTRICT: 3



APPLICANT'S NAME: <b>Gilbert Blilie, PLLC</b>		REQUESTED CHANGE:	
APPLICATION NO: <b>Z-104-K-75-3</b>		FROM: <b>PCD (Approved C-2 PCD) ( 3.84 a.c.)</b>	
DATE: <b>3/27/2024</b>		TO: <b>A-1 PCD ( 3.84 a.c.)</b>	
GROSS AREA INCLUDING 1/2 STREET AND ALLEY DEDICATION IS APPROX.		REVISION DATES:	
<b>3.84 Acres</b>			
AERIAL PHOTO & QUARTER SEC. NO.		ZONING MAP	
<b>QS 36-24</b>		<b>M-7</b>	
MULTIPLES PERMITTED		CONVENTIONAL OPTION	
<b>PCD (Approved C-2 PCD)</b>		<b>N/A (55)</b>	
<b>A-1 PCD</b>		<b>N/A</b>	
		* UNITS P.R.D OPTION	
		<b>N/A (67)</b>	
		<b>N/A</b>	

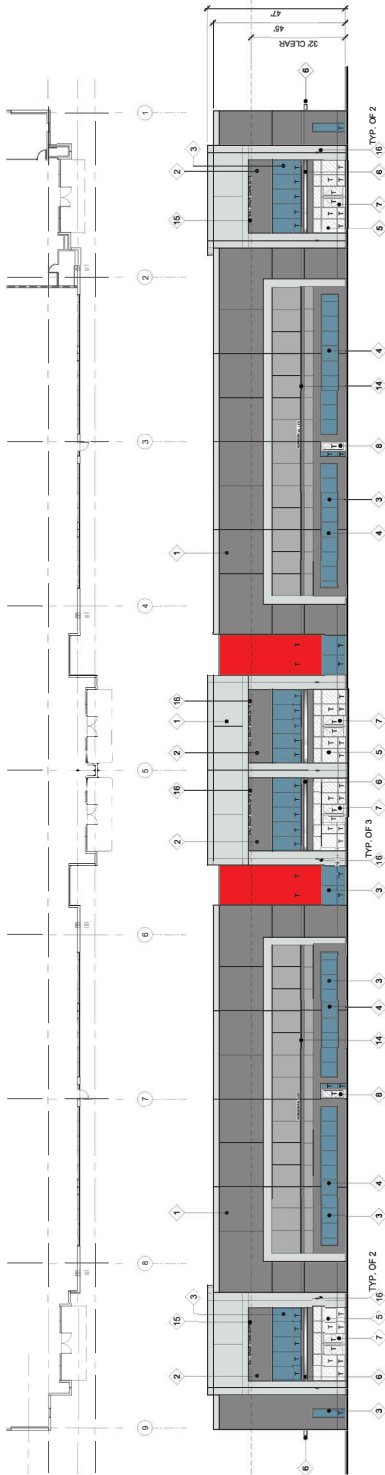
\* Maximum Units Allowed with P.R.D. Bonus



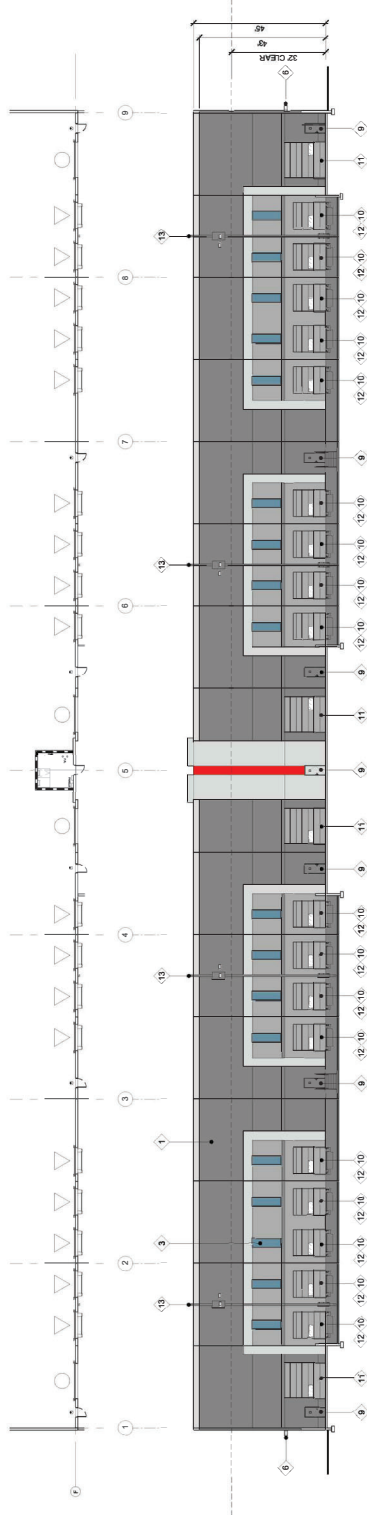




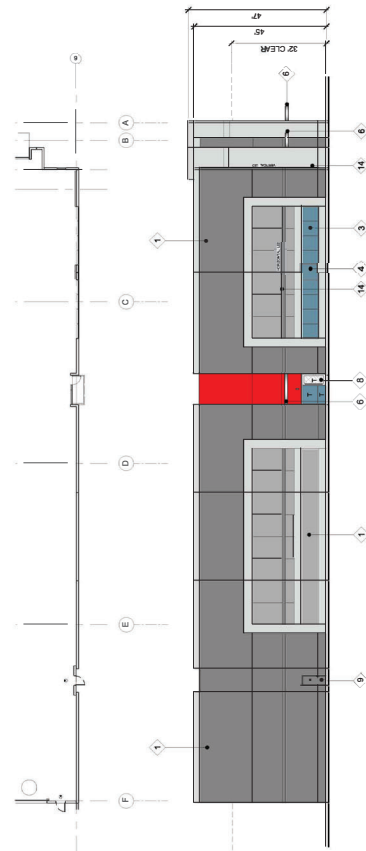


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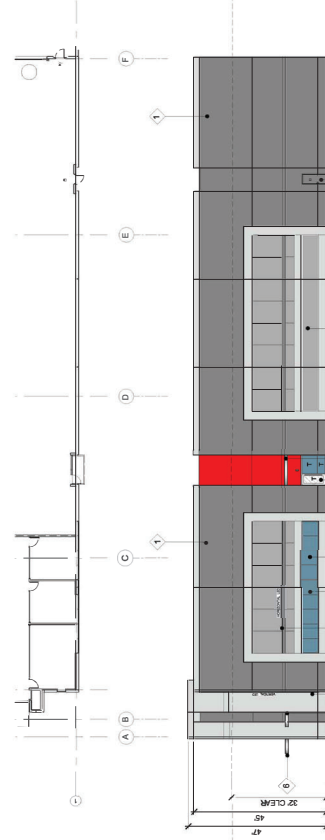
**1 BUILDING 7 NORTH ELEVATION**  
SCALE 1" = 20'-0"



## 2 BUILDING 7 SOUTH ELEVATION



#### 4 BUILDING 7 EAST ELEVATION



### 3 BUILDING 7 WEST ELEVATION